

North 101 Corridor

INSTITUTE OF GOVERNMENTAL
STUDIES LIBRARY

SEP 13 2004

UNIVERSITY OF CALIFORNIA



SPECIFIC PLAN

NORTH 101 CORRIDOR

SPECIFIC PLAN

Adopted
May 21, 1997

Ordinance No. 97-13

Effective Date
October 8, 1997

sponsored by:

**The City of Encinitas
Planning Commission and City Council**

prepared by:

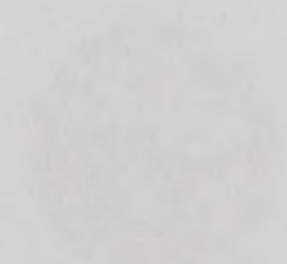
**City of Encinitas
Community Development Department**



Journal of the

Academy of Sciences

Volume 10, No. 1, 1960



CITY OF ENCINITAS

City Council

John Davis, Mayor
Lou Aspell, Deputy Mayor
Chuck DuVivier
James Bond
Sheila Cameron

Planning Commission

Alice Jacobson, Chair
Frank (Rusty) Wells, Vice-Chair
Lester H. Bagg
Robert Lanham
Anne Patton

This Plan was Prepared by:

Gary T. Barberio, Senior Planner
Beth A. Strehle, Planning Technician
Dave de Cordova, Management Analyst

Valuable Input From:

Sandy Holder, Community Development Director
Patrick Murphy, Consultant
Mark Hofman, Advanced Planning Intern
J. Alfred Dichoso, Advanced Planning Intern
J. Eric Culp, GIS Coordinator
Alan Archibald, Engineering Services Director
Hans Jensen, Senior Engineer
Lloyd Holt, Senior Engineer
Leroy Bodas, Associate Engineer
Blair Knoll, Associate Engineer
Rob Blough, Associate Traffic Engineer
Bill O'Donnell, Utility Planner
Will Foss, Building Division Supervisor
Corina Jimenez, Fire Prevention

Curtis Scott Englehorn, Environmental Consultant
Chris Schmidt, North County Transit District

ACKNOWLEDGEMENTS

We wish to extend special thanks to the following individuals who were involved in the North 101 Corridor Specific Plan process from its commencement.

Specific Plan Area Committee (SPArC)

Rick Smith
Albert Betti
Kirsten Richter
Patricia Fry
Douglas Harwood
Morgan Mallory
Sand Miller
Lloyd O'Connel
Delores Welty
Joan Herskowitz

Former Leucadia Community Advisory Board

Melissa Allen, Chair
Sheila Cameron, Vice-Chair
Marilyn Buck
Lars Fahlberg
John Eldon

Planning Commission

Lester H. Bagg, Chair
Alice Jacobson
Robert E. Lanham
Lee Rotscheck

City Staff

David de Cordova
Patrick Murphy

Special Editor

Lester H. Bagg

North 101 Corridor Specific Plan Table of Contents

<u>CHAPTERS</u>	<u>PAGE</u>
1.0 SUMMARY	1-1
1.1 INTRODUCTION	1-1
1.2 ENVIRONMENTAL ANALYSIS	1-9
2.0 COMMUNITY VISION AND SPECIFIC PLAN GOALS	2-1
3.0 USE AND DEVELOPMENT REGULATIONS	3-1
3.1 DEVELOPMENT STANDARDS	3-1
3.1.1 Residential Zones	3-4
3.1.2 Commercial Zones	3-6
3.1.3 Public/Semi-Public Zone	3-70
3.1.4 Historic Park Zone	3-70
3.1.5 Transportation Corridor Zone	3-71
3.1.6 Special Purpose Overlay Zones	3-71
3.2 PARKING	3-73
4.0 DESIGN REGULATIONS	4-1
4.1 INTRODUCTION AND PURPOSE	4-1
4.2 DEVELOPMENT REQUIRING DESIGN REVIEW	4-1
4.3 EXEMPTIONS FROM DESIGN REVIEW	4-2
4.4 APPEALS	4-2
4.5 COMMERCIAL AND MIXED USE DESIGN RECOMMENDATIONS	4-3
4.6 RESIDENTIAL DESIGN RECOMMENDATIONS	4-40
4.7 STREETScape CONCEPTS	4-44
4.8 GENERAL LANDSCAPE RECOMMENDATIONS	4-54
5.0 CIRCULATION PLAN	5-1
6.0 PUBLIC FACILITIES AND INFRASTRUCTURE	6-1
7.0 HISTORIC PRESERVATION	7-1
8.0 HOUSING	8-1
9.0 GENERAL PLAN AND LOCAL COASTAL PROGRAM COMPLIANCE	9-1
10.0 IMPLEMENTATION	10-1

Blank Page

List of Figures

<u>FIGURES</u>	<u>PAGE</u>
FIGURE 1: REGIONAL MAP	1-2
FIGURE 2: VICINITY MAP	1-3
FIGURE 3: ZONING MAP	3-3
FIGURE 4-A: NORTH HIGHWAY 101 AND NORTH VULCAN AVENUE STREETSCAPE CONCEPT	4-46
FIGURE 4-B: NORTH HIGHWAY 101 AND NORTH VULCAN AVENUE - TYPICAL CROSS-SECTION	4-47
FIGURE 4-C: NORTH HIGHWAY 101 PARKING ALCOVE CONCEPT	4-48
FIGURE 4-D: NORTH HIGHWAY 101 BUS TURNOUT CONCEPT	4-49
FIGURE 4-E: BUS STOP AREA AND SIDEWALK PLANTING POCKETS CONCEPT	4-50
FIGURE 4-F: LEUCADIA BOULEVARD / NORTH HIGHWAY 101 / NORTH VULCAN AVENUE INTERSECTION CONCEPT	4-51
FIGURE 4-G: LEUCADIA ROADSIDE PARK STREETSCAPE CONCEPT	4-52
FIGURE 4-H: NORTH 101 CORRIDOR COMPREHENSIVE STREETSCAPE CONCEPT PLAN	4-53
FIGURE 5-A: NORTH HIGHWAY 101 / LA COSTA AVENUE INTERSECTION RECOMMENDED LANE CONFIGURATION	5-8
FIGURE 5-B: NORTH VULCAN AVENUE / LA COSTA AVENUE INTERSECTION RECOMMENDED LANE CONFIGURATION	5-8
FIGURE 5-C: NORTH HIGHWAY 101 90-FOOT PROPOSED RIGHT-OF-WAY	5-9
FIGURE 5-D: NORTH VULCAN AVENUE - NORTH OF UNION STREET 55-FOOT PROPOSED RIGHT-OF-WAY	5-10
FIGURE 5-E: NORTH VULCAN AVENUE - SOUTH OF UNION STREET 50-FOOT PROPOSED RIGHT-OF-WAY	5-10
FIGURE 5-F: "A" STREET/SECOND STREET 80-FOOT PROPOSED RIGHT-OF-WAY	5-11
FIGURE 5-G: 56-FOOT PROPOSED RIGHT-OF-WAY	5-11
FIGURE 5-H: 50-FOOT PROPOSED RIGHT-OF-WAY	5-12
FIGURE 5-I: 45-FOOT PROPOSED RIGHT-OF-WAY	5-12
FIGURE 5-J: 40-FOOT PROPOSED RIGHT-OF-WAY	5-13
FIGURE 5-K: ALLEYWAY 15/20-FOOT PROPOSED RIGHT-OF-WAY	5-13

List of Tables

<u>TABLES</u>	<u>PAGE</u>
TABLE 1-1: ZONING COMPARISON	1-7
TABLE 1-2: SPECIFIC PLAN ZONING ACREAGE	1-8
TABLE 3-1: REQUIRED PARKING RATIOS	3-76
TABLE 5-1: LEVEL OF SERVICE - WITHIN SPECIFIC PLAN BOUNDARY	5-2
TABLE 5-2: LEVEL OF SERVICE - OUTSIDE SPECIFIC PLAN BOUNDARY	5-2
TABLE 5-3: STREET IMPROVEMENTS	5-4
TABLE 7-1: HISTORIC PRESERVATION STRATEGIES MIX	7-10

1.0 SUMMARY

1.1 INTRODUCTION

The preparation of this document is called for by the City's General Plan in recognition of the North 101 Corridor's unique character, needs, and opportunities. All components and requirements as specified in the General Plan have been addressed in this document which is made up of ten chapters:

1. Summary
2. Community Vision and Specific Plan Goals
3. Use and Development Regulations
4. Design Recommendations
5. Circulation Plan
6. Public Facilities and Infrastructure
7. Historic Preservation Plan
8. Housing
9. General Plan and Local Coastal Program Compliance; and;
10. Implementation.

The North 101 Corridor planning area consists of approximately 231 acres located within the communities of Leucadia and Old Encinitas in the City of Encinitas (Figure 1). The planning area is bounded by the City limit line on the north, B Street/Encinitas Boulevard on the south, parcels fronting Vulcan Avenue on the east, and parcels fronting North Highway 101 on the west, with some exceptions (Figure 2).

1.1.1 PLANNING BACKGROUND

From the early 1800's until the completion of Interstate 5 in the late 1960's, the unincorporated Leucadia and Encinitas areas evolved into unique beach-town communities with a small town atmosphere and picturesque setting. With easy access to a major freeway, these communities experienced tremendous growth that has generally continued to the present day. Accompanying this increase in prosperity, however, came the pressures and impacts of growth. The older, established beach-side communities have been experiencing transition, redevelopment and the redefinition of use and community function.

In 1986, Encinitas incorporated forming five diverse and unique communities: Cardiff, Leucadia, New Encinitas, Old Encinitas, and Olivenhain. The City's General Plan, which was adopted in 1989, calls for the preparation and implementation of this specific plan.

The primary purpose of the specific plan is to address the unique aspects, problems, and opportunities of the North 101 Corridor, and to maintain its identity, community character, and scale, while fostering the revitalization of the North Highway 101 commercial corridor.

REGIONAL MAP

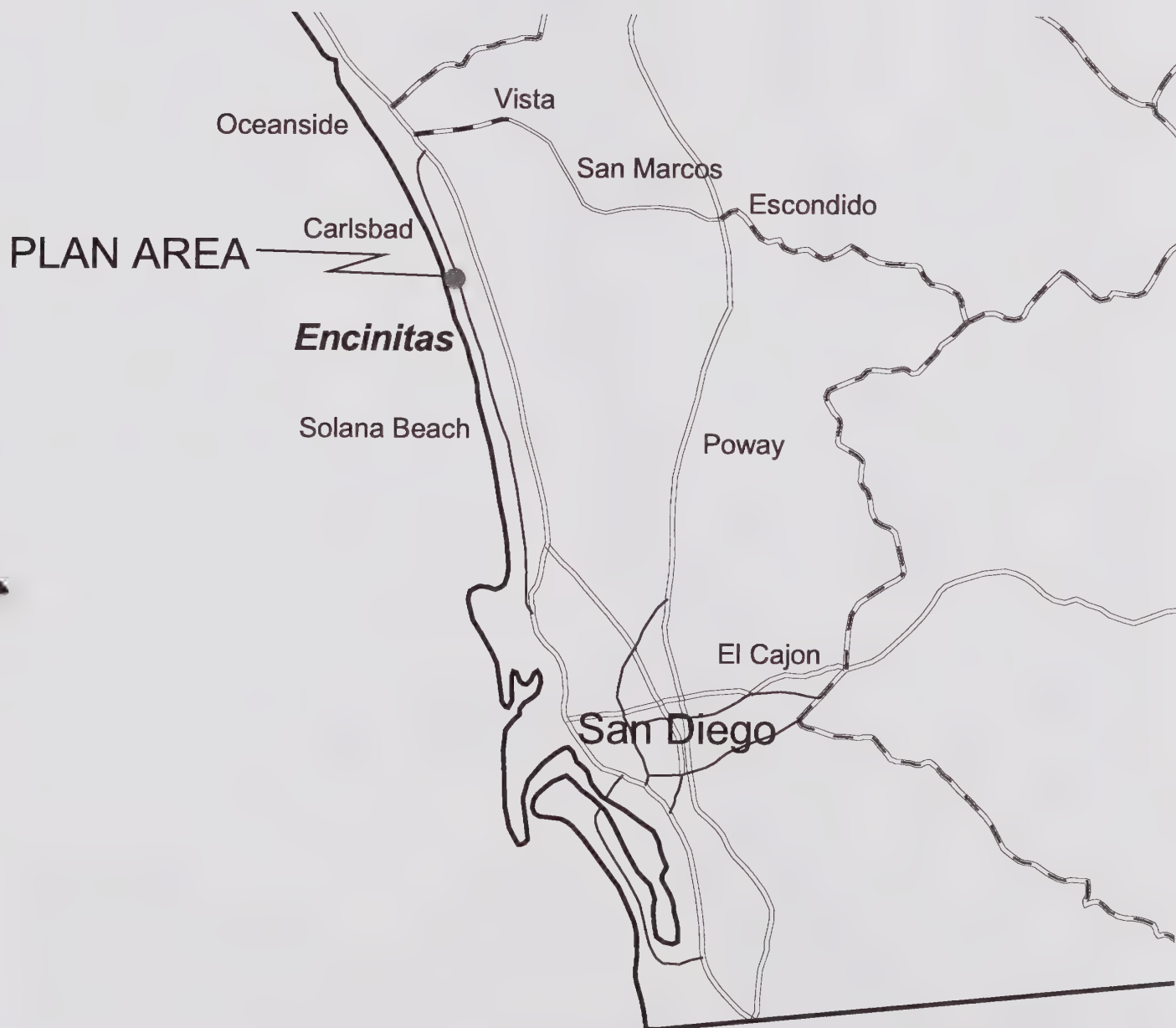


Figure 1
**North 101 Corridor
Specific Plan**

flavbimcdvregmap.apr

2 0 2 4 6 Miles



VICINITY MAP

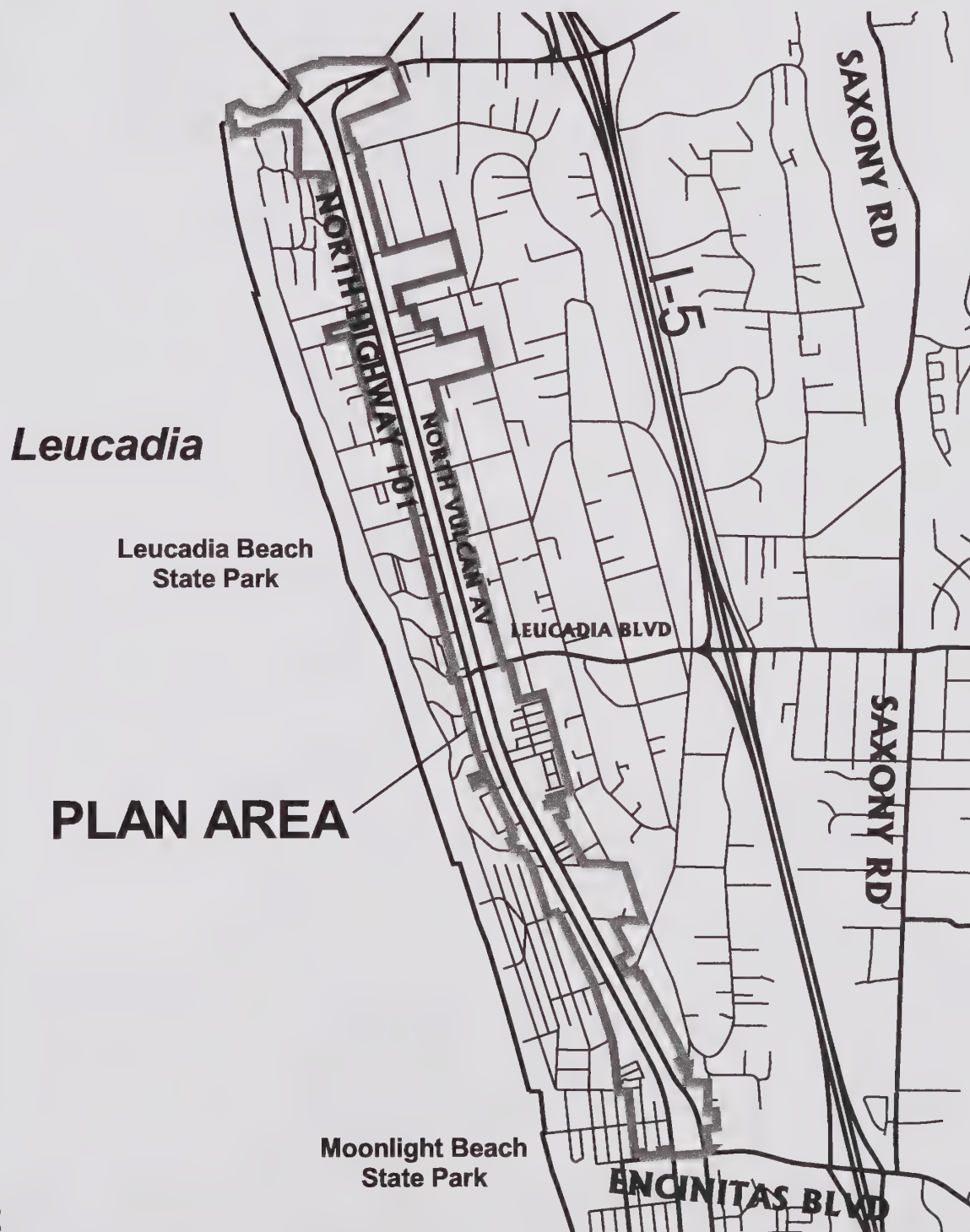


Figure 2
**North 101 Corridor
 Specific Plan**

f:\avb\ntcd\regmap\mapr

0.1 0 0.1 0.2 0.3 Miles



1.1.2 LAND USE

A primary goal of the specific plan is to maintain the unique and desirable aspects of the planning area, while providing continued private land use and investment, public improvements, and the economic success of the North Highway 101 Corridor Specific Plan area. In order to achieve this goal, the specific plan provides custom-tailored use and development regulations as described in Chapter 3.

A. Residential

1. Previous Zoning

Prior to the adoption of the North 101 Corridor Specific Plan, zoning in the plan area included a total of 77.0 acres designated as residential. This included 10.4 acres of Residential 3, 42.2 acres of Residential 8, 17.2 acres of Residential 11, 6.8 acres of Residential 25, and .4 acres of Residential Single-Family 11 as shown in Table 1-1.

2. Specific Plan

The specific plan allocates 83.1 acres of residential-only zoning which includes 10.4 acres of Residential 3 (N-R3), 28.4 acres of Residential 8 (N-R8), 1.4 acres of Residential 11 (N-R11), 4.9 acres of Residential 15 (N-R15), 15.8 acres of Residential 20 (N-R20), 10.6 acres of Residential 25 (N-R25), and 11.6 acres of Mobile Home Park (N-MHP) as shown in Table 1-1.

The existing Riviera Mobile Home Park totaling 11.6 acres, previously designated R-8, has been changed to a Mobile Home Park Zone (N-MHP) in order to recognize and help preserve its mobile home status. Another .4 acres previously zoned Residential Single-Family 11, located along North Highway 101, has been changed to the Commercial Mixed 1 Zone (N-CM-1), to be consistent with the property's frontage along Highway 101.

The specific plan has also expanded previous commercial zoning in the North Highway 101 Corridor Specific Plan area to allow residential use. Zones that were formerly designated as exclusively commercial have been changed to mixed use zones which allow residential as a secondary use in conjunction with commercial (N-CM-1, N-CM-2, and N-CM-3 Zones) or allow individual properties to develop as commercial, mixed use, or stand-alone residential (N-CRM-1 and N-CRM-2 Zones).

B. Commercial

1. Previous Zoning

Under previous zoning, commercial designations totaled 47.7 acres which included 39.0 acres of general commercial, 4.4 acres of limited visitor-serving commercial, 3.4 acres of visitor-serving commercial, and .9 acres of limited-local commercial as shown in Table 1-1.

2. Specific Plan

The specific plan provides a total of 53.3 acres of commercial and commercial mixed zoning which encompasses 13.7 acres of Commercial Mixed 1 (N-CM-1), 8.7 acres of Commercial Mixed 2 (N-CM-2), 3.3 acres of Commercial Mixed 3 (N-CM-3), 13.6 acres of Commercial, Residential Mixed 1 (N-CRM-1), 4.0 acres of Commercial, Residential Mixed 2 (N-CRM-2), 4.4 acres of Limited Visitor-Serving Commercial (N-L-VSC), and 5.6 acres of Visitor-Serving Commercial (N-VSC).

The N-CM-1, N-CM-2 and N-CM-3 Zones will allow residential as a secondary use to a commercial use. The N-CRM-1 and N-CRM-2 Zones will allow properties to develop as commercial, stand-alone residential or a mix of commercial and residential. These zones have been custom-tailored to reflect the existing development characteristics of the North 101 Corridor, which include small lots, zero front yard setbacks, and a higher percentage of lot coverage. The specific plan has reduced setback and landscape requirements that set forth flexible parking requirements and increased maximum percent lot coverage and Floor Area Ratios (FARs) from the citywide general commercial requirements. Refer to Section 3.1 for further discussion of these zones.

C. Office Professional

1. Previous Zoning

A total of 10.2 acres zoned office professional was previously located along Vulcan Avenue as shown in Table 1-1.

2. Specific Plan

The specific plan does not include any property designated office professional. 4.0 acres of office professional has been changed to the N-CRM-2 Zone, .5 acres to the N-CM-1 Zone, 4.1 acres to the N-R15 Zone and 1.6 acres to the N-R25 Zone..

D. Public/Semi-Public

1. Previous Zoning

A total of 8.4 acres previously zoned public/semi-public, located along Vulcan Avenue, included the Paul Ecke Central School site (7.5 acres) and the San Dieguito Unified High School District property (.9 acres) as shown in Table 1-1.

2. Specific Plan

The 7.5-acre Paul Ecke Central Elementary School site will remain a Public/Semi-Public Zone (N-P/SP). The remaining .9 acres, property used by the high school district for non-school uses, has been changed to Residential 15 (N-R15).

E. Ecological Resource/Open Space/Parks

1. Previous Zoning

Leucadia Roadside Park, comprising only .2 acres, was previously zoned ecological resource/open space/parks.

2. Specific Plan

Leucadia Roadside Park will remain a park site. However, the specific plan designates this site as a Historic Park Zone (N-HP). Refer to Section 3.1 for further discussion regarding this zone.

F. Rail Corridor

1. Previous Zoning

Uses in the railroad corridor, consisting of 33.2 acres, were not covered under previous citywide zoning regulations. However, this area has a Transportation Corridor Land Use designation under the City's General Plan.

2. Specific Plan

The railroad corridor is designated under the Transportation Corridor Zone (N-TC) which identifies specific uses permitted in this zone. A .50 acre portion of the railroad right-of-way at the northernmost boundary of the specific plan is now zoned as Visitor-Serving Commercial, reducing the Transportation Corridor Zone to 32.7 acres.

The specific plan is proposing a linear park along the railroad right-of-way, which will include a multi-modal recreational path, landscaping and parking bays. This could potentially increase the amount of open space within the specific plan area by approximately 18.6 acres.

Table 1-1

Zoning Comparison

Previous Zoning	Acres	Specific Plan Zoning	Acres
Residential 3 (R-3)	10.4	Residential 3 (N-R3)	10.4
Residential 8 (R-8)	42.2	Residential 8 (N-R8)	28.4
		Residential 25 (N-R25)	2.2
		Mobile Home Park (N-MHP)	<u>11.6</u>
			42.2
Residential 11 (R-11)	17.2	Residential 20 (N-R20)	15.8
		Commercial, Residential Mixed 1 (N-CRM-1)	<u>1.4</u>
			17.2
Residential 11 (RS-11)	.4	Commercial Mixed 1 (N-CM-1)	.4
Residential 25 (R-25)	6.8	Residential 25 (N-R25)	6.8
Office Professional (OP)	10.2	Residential 15 (N-R15)	4.1
		Residential 25 (N-R25)	1.6
		Commercial Mixed 1 (N-CM-1)	.5
		Commercial, Residential Mixed 2 (N-CRM-2)	<u>4.0</u>
			10.2
Limited Local Commercial (LLC)	.9	Commercial Mixed 1 (N-CM-1)	.9
Visitor-Serving Commercial (VSC)	3.4	Visitor-Serving Commercial (N-VSC)	2.2
		Residential 11 (N-R11)	<u>1.2</u>
			3.4
Limited Visitor-Serving Commercial (L-VSC)	4.4	Limited Visitor-Serving Commercial (N-L-VSC)	4.4
General Commercial (GC)	39.0	Commercial Mixed 1 (N-CM-1)	11.9
		Commercial Mixed 2 (N-CM-2)	8.7
		Commercial Mixed 3 (N-CM-3)	3.3
		Commercial, Residential Mixed 1 (N-CRM-1)	12.2
		Visitor-Serving Commercial (N-VSC)	<u>2.9</u>
			39.0
Public/Semi-Public (P/SP)	8.4	Public/Semi-Public (N-P/SP)	7.5
		Residential 15 (N-R15)	<u>.9</u>
			8.4
Ecological Resource/Open Space/Parks (ER/OS/P)	.2	Historic Park (N-HP)	.2
Transportation Corridor ¹	33.2	Transportation Corridor (N-TC)	32.7
		Visitor-Serving Commercial (N-VSC)	<u>.5</u>
			33.2
Streets	54.8	Streets	54.8
Total	231.5	Total	231.5

¹ General Plan Land Use Designation

Table 1-2

Specific Plan Zoning Acreage

Specific Plan Zone	Total Acres
Residential 3 (N-R3)	10.4
Residential 8 (N-R8)	28.4
Residential 11 (N-R11)	1.2
Residential 15 (N-R15)	5.0
Residential 20 (N-R20)	15.8
Residential 25 (N-R25)	10.6
Mobile Home Park (N-MHP)	11.6
Commercial Mixed 1 (N-CM-1)	13.7
Commercial Mixed 2 (N-CM-2)	8.7
Commercial Mixed 3 (N-CM-3)	3.3
Commercial, Residential Mixed 1 (N-CRM-1)	13.6
Commercial, Residential Mixed 2 (N-CRM-2)	4.0
Visitor-Serving Commercial (N-VSC)	5.6
Limited Visitor-Serving Commercial (N-L-VSC)	4.4
Public/Semi-Public (N-P/SP)	7.5
Historic Park (N-HP)	.2
Transportation Corridor (N-TC)	32.7
Streets	54.8
TOTAL	231.5

1.2 ENVIRONMENTAL ANALYSIS

As required by the California Environmental Quality Act (CEQA), an environmental impact report (EIR) was prepared addressing both the Downtown Encinitas and North 101 Corridor Specific Plans. This EIR was certified by the Encinitas City Council with the adoption of the Downtown Encinitas Specific Plan, and has been updated by an addendum making minor changes to reflect changes from the first draft to the final draft of the North 101 Corridor Specific Plan. The EIR examines a range of possible environmental effects resulting from the full implementation of both specific plans; that is, from build-out of land uses pursuant to specific plan zoning and design criteria, and from the completion of streetscape and capital facilities improvements.

Negative impacts are cited for certain resource issues including potential land use conflicts, noise, air quality, traffic circulation, and greater demand on library and school services. Mitigation measures to reduce these impacts to a level of insignificance are identified for all issues except cumulative regional air quality, and cumulative demand for schools and libraries. Related findings of the Statement of Overriding Considerations address these circumstances. The EIR also finds several positive impacts, including increased housing opportunities, preservation and enhancement of community character, and promotion of public safety by the completion of the street and infrastructure improvements. The EIR and Addendum are on file with the City, and may be referenced for the discussion of mitigation measures and details of environmental analysis.

2.0 COMMUNITY VISION AND SPECIFIC PLAN GOALS

2.1 COMMUNITY VISION

The Leucadia community is characterized by local, family-owned businesses; small-lot subdivisions; small-scale development and uses; and architectural diversity. The purpose of this specific plan is to help guide rehabilitation and successful economic restructuring of the North 101 Corridor as it evolves into the twenty-first century.

The community vision seeks to:

- promote a community where individuals can afford to live, as well as own and operate commercial establishments;
- encourage architectural quality and creativity without discouraging development or dictating a specific style or theme;
- establish a streetscape enhancement program along the North 101 corridor;
- provide a commercial corridor that serves both the needs of the neighborhood residents as well as visitors to the community;
- encourage diverse, small-scale uses and family-owned businesses;
- preserve and maintain the existing mature eucalyptus and cypress trees and establish a street tree program for the entire North 101 corridor;
- provide for safe pedestrian and traffic circulation;
- consider opportunities for mixed-use development; and
- to enhance the artistic community.

2.2 SPECIFIC PLAN GOALS

The specific plan sets forth the following goals:

2.2.1 LAND USE

- A. Establish design guidelines and development regulations that encourage diverse, small-scale uses and family owned or operated businesses along the North 101 corridor.
- B. Allow consolidation of adjacent parcels providing the goals of the specific plan are met.
- C. Provide for flexible land use opportunities such as mixed-use.
- D. Provide a balance between neighborhood-serving and visitor-serving uses.

- E. Identify land use opportunities for under-utilized land.
- F. Encourage architectural diversity and a unique character along North Highway 101.
- G. Encourage outdoor spaces for sidewalk cafes, street vendors, and other pedestrian oriented activities along North Highway 101.
- H. Enhance the overall image and streetscape in order to attract more visitors and shoppers to the corridor.
- I. Encourage land use buffers between incompatible uses such as commercial frontage adjacent to residential development.
- J. Promote and enhance artisan resources in the community.

2.2.2 HOUSING

- A. Provide affordable housing opportunities.
- B. Establish development guidelines that encourage high quality housing.

2.2.3 CIRCULATION

- A. Provide for safe pedestrian circulation.
- B. Improve parking opportunities.
- C. Improve vehicular traffic circulation.
- D. Promote and encourage the use of public transportation.

2.2.4 INFRASTRUCTURE & PUBLIC SAFETY

- A. Eliminate flooding and improve drainage.
- B. Underground utilities and provide more lighting.

2.2.5 RECREATION/OPEN SPACE

- A. Provide more parks and open space.

3.0 USE AND DEVELOPMENT REGULATIONS

The North 101 Corridor Specific Plan area has been divided into separate zones which are described below, and shown in Figure 3. Within each zone, development standards unique to its needs and circumstances have been devised, and differ from "citywide" zoning standards as required.

3.1 DEVELOPMENT STANDARDS

All applicable provisions of Title 30 of the Encinitas Municipal Code shall apply to all properties within the North 101 Corridor Specific Plan area, except as otherwise specified herein.

Zones

Distinct zones were created through the specific plan process, each with its own special development standards. The following provides a listing of each zone.

N-R3	Residential 3
N-R8	Residential 8
N-R11	Residential 11
N-R15	Residential 15
N-R20	Residential 20
N-R25	Residential 25
N-MHP	Mobile Home Park
N-CM-1	Commercial Mixed - 1
N-CM-2	Commercial Mixed - 2
N-CM-3	Commercial Mixed - 3
N-CRM-1	Commercial, Residential Mixed -1
N-CRM-2	Commercial, Residential Mixed -2
N-VSC	Visitor-Serving Commercial
N-L-VSC	Limited Visitor-Serving Commercial
N-P/SP	Public/Semi-Public
N-HP	Historic Park
N-TC	Transportation Corridor

Zones and Development Standards

In order to classify, designate, regulate and restrict the uses of land, buildings, and other structures to achieve the purposes of this Specific Plan and of the General Plan of the City of Encinitas, the zones and development standards described below are minimum standards unless otherwise stated.

The following development standards shall apply to all properties within the North 101 Corridor Specific Plan area. Standards detailed in Title 30 of the Encinitas Municipal Code shall apply, except as otherwise specified herein. If there is a conflict between the development standards of this Chapter and Title 30 of the Encinitas Municipal Code, the development standards of this Chapter shall take precedence.

In the event a proposed use is not specifically listed as allowed under a zone, but is similar in character to a use which is listed, a determination of allowable use pursuant to Section 30.01.030 of Title 30 of the Encinitas Municipal Code may be requested.

FIGURE 3

ZONING MAP

North 101 Corridor Specific Plan

Legend


-  Specific Plan Boundary
-  Residential 3 (N-R3)
-  Residential 8 (N-R8)
-  Residential 11 (N-R11)
-  Residential 15 (N-R15)
-  Residential 20 (N-R20)
-  Residential 25 (N-R25)
-  Mobile Home Park (N-MHP)
-  Commercial Mixed 1 (N-CM-1)
-  Commercial Mixed 2 (N-CM-2)
-  Commercial Mixed 3 (N-CM-3)
-  Comm. Res. Mixed 1 (N-CRM-1)
-  Comm. Res. Mixed 2 (N-CRM-2)
-  Visitor-Serving Commercial (N-VSC)
-  Ltd. Visitor-Serving Comm. (N-LVSC)
-  Public/Semi-Public (N-P/SP)
-  Historic Park (N-HP)
-  Transportation Corridor (N-TC)

Figure 3

Zoning Map

500 0 500 1000 1500 Feet



This map was produced by the City of Encinitas GIS Division and is designed for internal use only. The map is based on the best data available at the time of production and is not guaranteed to survey accuracy. City of Encinitas, RUIS, SANDAG and other data may be represented. Proprietary information: Access to and use of this information is restricted by a sublicense agreement. No sale, transfer license, or assignment of this information is permitted.



Figure 3: Zoning Map

3.1.1 RESIDENTIAL ZONES

**A. Zones: Residential 3 (N-R3)
Residential 8 (N-R8)
Residential 11 (N-R11)
Residential 15 (N-R15)
Residential 20 (N-R20)
Residential 25 (N-R25)**

1. Except as otherwise specified below, all provisions of Title 30 of the Encinitas Municipal Code applicable to the R-3, R-8, R-11, R-15, R-20 and R-25 Zones shall apply to each respective residential zoning classification listed herein.
2. The Landscape Recommendations of Section 4.8 of this Specific Plan shall apply to all new and rehabilitated landscape projects with a landscaped area of greater than 1,000 square feet.
3. Applicable provisions of the Specific Plan Design Recommendations, Chapter 4 of this Specific Plan, shall apply to proposed development under these zones. If a conflict arises between provisions of the design recommendations and the development standards of these zones, the development standards shall take precedence.
4. All detached or attached dwelling units in residential-only developments must be constructed on a legally subdivided lot or must be subdivided to permit ownership of airspace in the form of a dwelling unit with an undivided share in common elements.
5. The building height standard for residential developments shall be consistent with Section 30.16.10.B.7 of Title 30 of the Encinitas Municipal Code except as specified below:

Residential developments may be approved to a maximum height of 33 feet or 3 stories, whichever is less, provided the first story is used for garage or parking purposes for residents of the development. The only portion of the building that may be approved at the 33 feet or 3 story height limit is that portion of the building situated directly above the first story garage or parking area.

The intent of this provision is to allow for two residential stories above a first story garage or parking area. All other portions of the building that are not directly situated above a first story garage or parking area shall meet all residential building height standards of Section 30.16.10.B.7 of Title 30 of the Encinitas Municipal Code. In all cases, the height of a structure shall be determined pursuant to Section 30.16.10.B.7.f. of the Title 30 of the Encinitas Municipal Code.

B. Zone: Mobile Home Park (N-MHP):

This zone is intended to provide for mobile home park development and redevelopment for new or redeveloped parks greater than 5 acres.

1. Except as otherwise specified below, all use and development standards pursuant to Section 30.16.030, Subsections 1-10, of Title 30 of the Encinitas Municipal Code shall apply under the N-MHP zone.
2. The maximum allowable density under the N-MHP zone is 15 units per net acre.
3. The Landscape Recommendations of Section 4.8 of this Specific Plan shall apply to all new and rehabilitated landscape projects with a landscaped area of greater than 1,000 square feet.
4. Applicable provisions of the Specific Plan Design Recommendations, Chapter 4 of this Specific Plan, shall apply to proposed development under this zone. If a conflict arises between provisions of the design recommendations and the development standards of this zone, the development standards shall take precedence.

3.1.2 COMMERCIAL ZONES**A. Zone: Commercial Mixed 1 (N-CM-1)**

This zone is intended to provide a zoning district where the development of general commercial uses, excluding auto-related uses, and mixed use may be allowed. The zone provides for a variety of development opportunities including 1) stand-alone commercial; or 2) mixed use, with general commercial use types as listed in this chapter. The N-CM-1 zone applies to properties with frontage on North Highway 101, generally between Marcheta Street and West Glaucus Street, as well as commercial properties located at the intersection of Leucadia Boulevard and Vulcan Avenue and commercial properties located on the east side of North Highway 101, just north of Encinitas Boulevard. The intent of this zone is to achieve an eclectic blend of uses and architectural styles by orienting buildings to North Highway 101, emphasizing a pedestrian scale and friendliness, and continuing to emphasize the use of dominating trees in the streetscape.

Commercial allows a wide range of general commercial activities, including retailing, service, and visitor-serving uses, with the intent of accommodating citywide or regional needs and serving the needs of persons visiting the city for business and recreational purposes.

Mixed Use allows commercial and residential uses at a maximum density of 25.0 dwelling units per net acre on the same property or in the same structure, with the intent of providing opportunities for housing and live/work or artisan loft arrangements, while also mitigating the impacts between commercial and residential uses.

It is the intention of the N-CM-1 Zone to allow for functional and physical integration of project components of mixed uses. Consideration will be given to joint use of parking, sizes of residential units, and overall architectural design when considering approvals for mixed use developments.

The following principal uses shall be permitted in the N-CM-1 Zone and are identified as either permitted, or requiring a minor use permit or major use permit. All other principal uses not specifically listed are prohibited.

1. Permitted Uses:

- Accessory Building
- Antique Sales, Retail
- Appliance Sales & Repairs (Household and Small Appliances)
- Aquarium, Commercial
- Arcade Accessory (Non-Adult)
- Art Gallery
- Artisan/Craftsman (Sales & Studio)
- Bakery (Retail)
- Bank/Savings and Loan
- Barber and Beauty Shop (Cosmetologist)
- Bicycle Sales, Rental and Service
- Billiard, Pool Hall
- Blueprinting and Photostatting
- Bookbinding
- Book Sales

3.0 Use and Development Regulations

Bowling Alley
Building Materials Sales, Enclosed
Candle Shop
Candy and Confectionery Sales
Catering Service
Charitable Bins and Depositories (accessory)
Clothing Rental
Coins, Purchase and Sales
Computer Sales & Service
Conservatory of Music
Convenience Store
Cosmetic Design Studio
Costume Rentals
Dairy Store
Dance Hall
Dance Studio
Delicatessen
Dental Clinic
Dressmaking Shop
Drug Store
Dry Cleaning, Retail and Self-Service
Dwelling Unit, Caretaker -accessory to the principal use
Dwelling Unit, One-Family¹
Dwelling Unit, Two-Family¹
Dwelling Unit, Three-Family¹
Dwelling Unit, Multiple Family¹
Educational Institution, Public
Employment Agency
Family Day Care Home, Small (Six or fewer persons, accessory to a dwelling only)
Family Day Care Home, Large (Seven to twelve persons, accessory to dwelling only)
Finance Company
Fire Prevention Equipment, Sales and Service
Fire Station
Floor Covering, Retail
Florist Shop
Furniture Sales
Garden Supplies
Gem/Precious Stone Retail
Gift Shop
Glass and Mirrors, Retail
Glass Edging and Beveling
Glass Studio (Stained and others)
Grocery Store
Group Home¹
Hair Salon
Hardware Store
Hat Shop
Health Food Store
Hobby Supply Shop
Home & Business Maintenance Service
Home Occupations
Ice Cream Parlor
Interior Decorating Service
Janitorial Services
Janitorial Supplies/Sales

¹ Allowed in mixed-use developments only. All detached or attached dwelling units in mixed-use developments must be constructed on a legally subdivided lot or must be subdivided to permit ownership of airspace in the form of a dwelling unit with an undivided share in common elements.

Jewelry Sales
Lapidary Shop, Retail (Gems)
Laundromat
Laundry
Lawnmower Sales and Service
Library
Locksmith
Machine and Tool Sales
Mail Order House
Market (food)
Medical/Dental Clinic
Medical/Dental Office
Medical Equipment Sales
Museum
Music/Record Store
Newsstand
Notions and Dry Goods Store
Office (Business and Professional)
Office Equipment/Supplies Sales
Optical Products Sales
Orthopedic Devices Sales
Paper Product Sales
Parcel Delivery Service, Office only
Pet Shop/Pet Grooming
Pharmacy
Photocopy Shop
Photofinishing, Retail
Photographic Studio
Photographic Supplies
Photograving and Finishing
Post Office
Postal Annex, Private Ownership
Pottery Sales
Printing
Public Utilities: Office
Radio/Television Sales
Real Estate Office
Reducing Salon
Restaurant (no alcohol sales; outdoor dining permitted by design review)¹
Retail Sales²
Schools, Public: (Elementary, Jr. High, High School)
Scientific Instruments Sales
Second Hand Dealer
Second Hand Store
Shoe Repair/Sales
Sidewalk Dining (requires encroachment permit)¹
Sign Shop
Silk Screen Printing Shop
Sporting Goods Sales
Stationery Store
Surf Shop Retail and Rental
Swimming Pool Supplies/Equipment Sales
Tailor Shop
Telephone Answering Service

¹ See Section A.4.d Sidewalk Dining in this chapter for additional provisions.

² Retail sales uses consistent with the intent of the N-CM-1 Zone are permitted.

Theaters and Places of Public Assembly
Tile Sales
Utility Lines - Sewer, Gas, Petroleum, Telephone, Electric
Veterinarian (without Kennel)
Video Sales & Rental
Wearing Apparel Shop
Wood Product Sales, Retail

2. Minor Use Permits:

Alcoholic Beverage, Sales - off premise
Auction House
Bed & Breakfast
Cabinet Shop
Club, Athletic or Recreational
Club, Private
Courts, Commercial (Badminton, Tennis, Racquetball, Others)
Day Care Center
Gymnasium, Public Health Club
Horticultural Services
Laboratories (biochemical, film, other)
Laboratories (medical, dental)
Nurseries, Horticultural
Outdoor Sales
Packaging Business
Parks and Recreation Areas
Pawnshop
Recording Studio
Recreational Facilities Private
Recreational Facilities Public
Restaurant (with alcohol sales; outdoor dining permitted)¹
Small Animal Hospital/Kennel
Video Production Studio
Wholesale Business²

¹ See Section A.4.d Sidewalk Dining in this chapter for additional provisions.

² Wholesale businesses consistent with the intent of the N-CM-1 zone

3. Major Use Permits:

Athletic Field
Bar/Cocktail Lounge
Cellular Facility
Church/Temple/Religious Institution
Club, with Alcohol Sales
Educational Institution, Private
Electrical Distribution Substation
Emergency Residential Shelter
Garage, Public Parking
Gas Distribution, Meter and Control Station
Hotel
Massage Parlor (see Adult Use 30.51, Title 30 of the Encinitas Municipal Code)
Medical Complex
Motel
Parking Lot, Commercial
Police/Sheriff Station
Radio/Television Broadcasting Studio
Radio/Television Transmitter
Schools, Private: (Elementary, Jr. High, High School)
Schools, Technical
Swap Meet
Time Share
Transitional Housing

4. Development Standards - Commercial or Mixed-Use Developments:

a. General Standards:

Net Lot Area:	6,000 net square feet minimum
Lot Width:	75 feet
Lot Depth:	80 feet
Front Yard Setback:	0 feet ¹
Side Yard Setback for each interior side:	0 feet ²
Side Yard Setback street side:	0 feet ²
Rear Yard Setback:	0 feet ^{2/3}
Lot Coverage (maximum):	90 percent
Building Height maximum:	33 feet or three stories, whichever is less.
Landscaping:	10 percent ⁴ (See Landscape Recommendations, Section 4.8)
Off-Street Parking:	See Parking Requirements, Section 3.3
Floor Area Ratio:	No requirement

b. Mixed-Use Site Planning:

1. Separate entrances shall be required for residential and commercial uses when occupying the same structure. (This does not preclude secondary interior access between commercial and residential uses, in addition to the required separate accesses, provided that residential access to or through commercial uses avoids unsafe commercial areas.)
2. Residential uses shall be located either above or behind the primary commercial use and shall not occupy the ground floor building frontage area along Highway 101.
3. All detached or attached dwelling units in mixed-use developments must be constructed on a legally subdivided lot or must be subdivided to permit ownership of airspace in the form of a dwelling unit with an undivided share in common elements.
4. Residential uses shall not exceed 50 percent of the gross building floor area for the development site, unless authorized through the approval of a Major Use Permit.
5. Residential densities shall be calculated pursuant to Section 30.16.010.B 1. and 2. of Title 30 of the Encinitas Municipal Code.

¹ Lots with frontage on Melrose Avenue or La Veta Avenue, which do not extend through to Highway 101, shall maintain a twenty (20) foot front yard setback from Melrose/La Veta.

² A minimum ten (10) foot rear or side yard setback shall be required where any portion of the rear or side yard immediately abuts a residentially zoned property.

³ Lots with frontage on North Highway 101 which extend through to Melrose Avenue or La Veta Avenue, shall maintain a minimum twenty (20) foot rear yard setback from Melrose Avenue or La Veta Avenue.

⁴ Pedestrian plazas or decorative landscaping, which include site amenities such as sculpture, fountains, planters, enhanced paving, etc., may be counted towards meeting the respective landscaping minimum, up to a maximum of 50 percent of the respective landscaping minimum.

c. Additional Setbacks and Separation from Adjacent Land Uses:

1. Where a commercial or mixed-use development abuts property in any residential zone, a masonry wall of no less than six (6) feet in height is required along the property line between the commercial/mixed use development and the residentially-zoned property, and may be increased to eight (8) feet to resolve any noise or visual impacts. Said wall shall be measured from the highest finished grade at the property line, and shall be erected and maintained as separation between the commercial and residential uses. Said wall shall be stepped down to no more than four (4) feet in height within 10 feet of the front property line.
2. For lots fronting along North Highway 101 which extend through to Melrose Avenue or La Veta Avenue, a minimum landscape area of ten (10) foot in depth shall be required along Melrose Avenue or La Veta Avenue. A masonry wall of no less than six (6) feet in height, which may be increased to eight (8) feet to resolve any noise or visual impacts, measured from the highest finished grade at the property line shall be erected along the landscape area, located a minimum of ten (10) feet from the property line.

d. Sidewalk Dining:

1. Outdoor/sidewalk dining areas that encroach into the public right-of-way shall require an encroachment permit from the Engineering Services Department and must maintain a minimum 4-foot clearance which excludes planter areas, fire hydrants, street lights, other street furniture, on-street auto parking overhang, and any approved A-frame signage.
2. Encroachment permits for outdoor/sidewalk dining within public rights-of-way require the following information and are subject to the following limitations:
 - a. Submit a brief description of the proposed encroachment and its proposed location in the public right-of-way.
 - b. Submit a site plan, in a form acceptable to the Engineering Services Department and the Community Development Department, showing local conditions, including street and sidewalk width, and location/dimensions of all street furniture and elements on the sidewalk.
 - c. Specify the number of tables and seating requested and not-to-exceed-amount.
 - d. An issued permit may be revoked by the City after a 30-day notice.
 - e. Hours of operation shall be pre-determined through the encroachment permit and limited to an associated adjacent eating and drinking establishment.
 - f. No sound amplification device, musical instrument or sound reproduction device shall be operated or used with outdoor dining, and outdoor lighting shall comply with performance standards, and be approved by the City.

- g. Prior to issuance and approval of a permit, a finding shall be made by the Community Development Director or designee that the outdoor dining will not adversely affect the neighborhood nor be detrimental to persons residing, visiting or working in the area.
- h. A liability insurance policy naming the city as additionally insured for \$1,000,000 shall be on file with the Engineering Services Department and the Community Development Department.

e. General Site Planning:

- 1. Site planning shall include consideration of adjoining parcels in terms of building configuration, building design, and scale of landscaping materials, circulation/parking configuration and access.
- 2. All commercial uses, including storage, shall be located within an enclosed building unless authorized by a Minor Use Permit, or as otherwise permitted by this Specific Plan.

f. Building Design and Use:

- 1. Visual relief shall be provided for linear buildings by the use of architectural projections or recessed areas which create the appearance of smaller adjoining buildings.
- 2. Energy consumption in buildings should be minimized using the following features where feasible:
 - Cogeneration
 - Solar access
 - South facing windows with eave coverage
 - Double glazed windows
 - Deciduous shade trees
 - Good ventilation
 - Efficient lighting
 - Day lighting
- 3. All signage associated with the buildings and site shall be integrated in terms of both architecture and color and shall comply with Section 4.5.6 of the Specific Plan.

g. Access, Circulation, and Parking:

- 1. Vehicular access shall be taken from an abutting alley or adjacent street rather than from North Highway 101 where feasible.
- 2. Where vehicular access off of North Highway 101 is necessary, access shall be limited to one point for each 300 feet of frontage or one point per parcel if frontage is less than 300 feet.
- 3. All alleys shall be maintained for vehicular access and/or pedestrian paths, and shall not be vacated.
- 4. Access points to adjoining lots shall be shared wherever feasible. Reciprocal ingress and egress, circulation, and parking arrangements shall

be required to facilitate the ease of vehicular movement between adjoining properties where feasible;

5. Sidewalks shall be located along the west side of North Highway 101 and other street frontages per streetscape concepts and circulation improvement standards under this Specific Plan;
6. Where feasible, on-site parking areas shall be located off the alley or to the rear of a building. On-site parking areas abutting all local roads or North Highway 101 shall be screened by earth mounding, landscaping, low decorative walls, or a combination of any of these methods, three (3) feet in height.
7. For lots fronting along North Highway 101 which extend through to La Veta Avenue or Melrose Avenue, vehicular access from Melrose Avenue or La Veta Avenue shall be prohibited unless the location of existing structures to continue in use prohibits access from an abutting alley, side street or Highway 101. Where access from Melrose Avenue or La Veta Avenue must be provided, access is limited to no more than one point for each 300 feet of frontage or one point per parcel where frontage is less than 300 feet.

h. Auxiliary Structures/Equipment and Utilities:

1. All roof appurtenances including, but not limited to, air conditioning units, and mechanical/electrical equipment shall be shielded and architecturally screened from view from on-site parking areas, adjacent public streets and adjacent properties.
2. All ground-mounted mechanical/electrical equipment, including heating and air conditioning units, and trash receptacle areas, shall be completely screened from surrounding properties by use of a wall, fence, or landscaping, or shall be enclosed within a building.
3. All utility connections shall be designed to coordinate with the architectural elements of the site so as not to be exposed except where necessary. Pad-mounted transformers and/or meter box locations shall be included in the site plan with an appropriate screening treatment. All new and existing utility connections within the boundaries of the project shall be placed underground, with the exception of existing overhead power transmission lines with voltage levels exempted by the Municipal Code and long distance and main trunk communications facilities. Transformer, terminal boxes, meter cabinets, pedestals, ducts and other facilities may be placed above ground provided they are screened.
4. Trash receptacles shall be located behind the building, where possible. In all locations, trash receptacles shall be enclosed by a six-foot high decorative masonry wall with view-obstructing gates; for lots with frontage extending through to Melrose Avenue or La Veta Avenue, trash storage shall be behind the twenty (20) foot setback required from Melrose Avenue or La Veta Avenue.
5. Outdoor storage areas, when approved by minor use permit, shall be located to the rear of a building and shall be enclosed by solid decorative

masonry walls and view-obstructing gates, both to be not less than six (6) feet in height to adequately screen such areas from view. Reasonable substitutions such as masonry, wood or metal pilasters with wrought iron or vinyl-clad chain link and view obscuring material may be approved through Design Review.

6. For lots fronting along North Highway 101 which extend through to Melrose Avenue or La Veta Avenue, all auxiliary structures/equipment, and utilities shall be setback a minimum of twenty (20) feet from Melrose Avenue or La Veta Avenue.

i. Landscaping:

1. Site landscaping shall include a combination of trees, shrubbery, vines, or groundcovers all of which shall be drought-tolerant, where feasible. Landscaping in parking areas shall be predominantly trees to provide shade and visual relief.
2. Where feasible, landscaping in parking areas shall include one fifteen (15) gallon tree for every four (4) parking spaces distributed throughout the parking area.
3. All landscaped areas shall be watered by an automatic irrigation system with underground piping (excluding drip or soaker irrigation systems) and regularly maintained in a healthy and thriving condition free of weeds, trash, debris.
4. A minimum of 10 percent of the site area shall be landscaped. Street trees shall be provided, in accordance with the specific plan landscape recommendations. Pedestrian plazas or decorative landscaping, which include site amenities such as sculpture, fountains, planters, enhanced paving, etc., may be counted towards meeting the respective landscaping minimum, up to a maximum of 50 percent of the respective landscaping minimum.
5. Landscaping shall be in accordance with Section 4.8, Landscape Recommendations.

j. Lighting:

1. Lighting placed upon the building shall be architecturally integrated.
2. All light sources shall be shielded in such a manner that the light is directed away from streets or adjoining properties. Illuminators should be integrated within the architecture of the building. Freestanding lamp posts shall be no taller than eighteen (18) feet. The intensity of light at the boundary of the N-CM-1 Zone shall not exceed seventy-five (75) foot lamberts from the source of light.

k. Architectural Features, Accessory Structures and Walls/Fences:

1. Accessory structures shall not be located in front of or on the street side of the main building(s).

2. Accessory structures shall meet the setback requirements for main building(s).
3. Between any street property line and a principal structure on that property, no wall or fence shall exceed four (4) feet in height. Where a retaining wall is used to increase usable lot area, the height of said retaining wall between any street property line and the principal structure shall not exceed four (4) feet in height.
4. The use of chain link, barbed wire and razor wire fencing within public view shall be prohibited.

l. Condominium Conversion

The conversion of a project to condominium ownership shall meet all requirements of the zone to the maximum extent possible within the constraints of the existing development. In no case shall the requirements for the screening of storage/sales areas or mechanical equipment be waived.

m. Design Recommendations

Applicable provisions of the Specific Plan Design Recommendations, Chapter 4 of this Specific Plan, shall apply to proposed development under this zone. If a conflict arises between provisions of the design recommendations and the development standards of this zone, the development standards shall take precedence.

n. Signage Regulations

Except as otherwise specified in this Section and Chapter 4 of this Specific Plan, the signage standards, and review and approval procedures of Chapters 23.08 and 30.60 of the Encinitas Municipal Code shall apply for all signs within the North 101 Corridor Specific Plan.

o. Freestanding A-frame signage

In addition to other types of permitted signage, freestanding A-frame signs shall be allowed, subject to the following standards:

1. one (1) per business.
2. maximum five (5) feet in height by three (3) feet in width.
3. maximum of two (2) faces per sign, back-to-back.
4. no external or internal illumination.
5. not permanently affixed to any object, structure or the ground, including utility poles, light poles, trees or any merchandise or products displayed outside of buildings.
6. portable, self-supporting, stable and weighted or constructed to withstand overturning by wind or contact.

7. not on display during non-business hours.
8. located directly in front of the building/business it is advertising.
9. not placed in such a way as to interfere with pedestrian or vehicular sight lines or corner clear zone requirements as specified by the City.
10. not placed in such a way as to obstruct access to a public sidewalk, public street, driveway, parking space, fire door, fire escape, handicapped access or in such a way that it obstructs free passage over any public right-of-way.
11. not obscure or interfere with the effectiveness of any official notice or public safety device.
12. maintain at least a four (4) foot width for pedestrian clearance over the entire length of the sidewalk or the public right-of-way in front of the business, free of all obstructions such as planter areas, fire hydrants, street lights, other street furniture, on-street parking overhang, and any other approved public right-of-way dining area.

p. Placement of A-frame Signs in the Public Right-of-Way

In addition to meeting the above standards, A-frame signs which encroach into the public right-of-way are allowed with the issuance of an encroachment permit from the Engineering Services Department. Encroachment permits for A-frame signs in the public right-of-way shall require the submittal of the following information and are subject to the following limitations:

1. Submit a brief description of the proposed encroachment and its proposed location in the public right-of-way.
2. Submit a site plan, in a form acceptable to the Engineering Services Department and the Community Development Department, showing local conditions, including street and sidewalk width, and location/dimensions of all street furniture and elements on the sidewalk.
3. The description and site plan shall be reviewed and accepted by the Community Development Department prior to the issuance of an encroachment permit by the Engineering Services Department.
4. A liability insurance policy naming the City as additionally insured in the amount of \$1,000,000 shall be on file with the Engineering Services Department and the Community Development Department.
5. An issued encroachment permit may be revoked by the City after a thirty (30) day notice.

B. Zone: Commercial Mixed 2 (N-CM-2)

This zone is intended to provide a zoning district where the development of general commercial uses, including auto-related uses, and mixed use may be allowed. The zone provides for a variety of development opportunities including 1) stand alone commercial; or 2) mixed use, with general commercial use types as listed in this chapter. The N-CM-2 zone applies to properties with frontage on North Highway 101, generally between A Street and Marcheta Street. The intent of this zone is to achieve an eclectic blend of uses and architectural styles by orienting buildings to North Highway 101, emphasizing a pedestrian scale and friendliness, and continuing to emphasize the use of dominating trees in the streetscape.

Commercial allows a wide range of general commercial activities, including auto-related uses, retailing, service, and visitor-serving uses, with the intent of accommodating citywide or regional needs and serving the needs of persons visiting the city for business and recreational purposes.

Mixed Use allows commercial and residential uses at a maximum density of 25.0 dwelling units per net acre on the same property or in the same structure, with the intent of providing opportunities for housing and live/work or artisan loft arrangements, while also mitigating the impacts between commercial and residential uses.

It is the intention of the N-CM-2 zone to allow for functional and physical integration of project components of mixed uses. Consideration will be given to joint use of parking, sizes of residential units, and overall architectural design when considering approvals for mixed use developments.

The following principal uses shall be permitted in the N-CM-2 Zone and are identified as either permitted, or requiring a minor use permit or major use permit. All other principal uses not specifically listed are prohibited.

1. Permitted Uses:

- Accessory Building
- Antique Sales, Retail
- Appliance Sales & Repairs (Household and Small Appliances)
- Aquarium, Commercial
- Arcade Accessory (Non-Adult)
- Art Gallery
- Artisan/Craftsman(Sales & Studio)
- Bakery (Retail)
- Bank/Savings and Loan
- Barber and Beauty Shop (Cosmetologist)
- Bicycle Sales, Rental and Service
- Billiard, Pool Hall
- Blueprinting and Photostatting
- Bookbinding
- Book Sales
- Bowling Alley
- Building Materials Sales, Enclosed
- Candle Shop
- Candy and Confectionery Sales

3.0 Use and Development Regulations

Catering Service
Charitable Bins and Depositories (accessory)
Clothing Rental
Coins, Purchase and Sales
Computer Sales & Service
Conservatory of Music
Convenience Store
Cosmetic Design Studio
Costume Rentals
Dairy Store
Dance Hall
Dance Studio
Delicatessen
Dental Clinic
Dressmaking Shop
Drug Store
Dry Cleaning, Retail and Self-Service
Dwelling Unit, Caretaker -accessory to the principal use
Dwelling Unit, One-Family¹
Dwelling Unit, Two-Family¹
Dwelling Unit, Three-Family¹
Dwelling Unit, Multiple Family¹
Educational Institution, Public
Employment Agency
Family Day Care Home, Small (Six or fewer persons, accessory to a dwelling only)
Family Day Care Home, Large (Seven to twelve persons, accessory to dwelling only)
Finance Company
Fire Prevention Equipment, Sales and Service
Fire Station
Floor Covering, Retail
Florist Shop
Furniture Sales
Garden Supplies
Gem/Precious Stone Retail
Gift Shop
Glass and Mirrors, Retail
Glass Edging and Beveling
Glass Studio (Stained and others)
Grocery Store
Group Home¹
Hair Salon
Hardware Store
Hat Shop
Health Food Store
Hobby Supply Shop
Home & Business Maintenance Service
Home Occupations
Ice Cream Parlor
Interior Decorating Service
Janitorial Services
Janitorial Supplies/Sales
Jewelry Sales
Lapidary Shop, Retail (Gems)
Laundromat

¹ Allowed in mixed-use developments only. All detached or attached dwelling units in mixed-use developments must be constructed on a legally subdivided lot or must be subdivided to permit ownership of airspace in the form of a dwelling unit with an undivided share in common elements.

Laundry
Lawnmower Sales and Service
Library
Locksmith
Machine and Tool Sales
Mail Order House
Market (food)
Medical/Dental Clinic
Medical/Dental Office
Medical Equipment Sales
Motor Vehicle Parts Store
Museum
Music/Record Store
Newsstand
Notions and Dry Goods Store
Office (Business and Professional)
Office Equipment/Supplies Sales
Optical Products Sales
Orthopedic Devices Sales
Paper Product Sales
Parcel Delivery Service, Office only
Pet Shop/Pet Grooming
Pharmacy
Photocopy Shop
Photofinishing, Retail
Photographic Studio
Photographic Supplies
Photograving and Finishing
Post Office
Postal Annex, Private Ownership
Pottery Sales
Printing
Public Utilities: Office
Radio/Television Sales
Real Estate Office
Reducing Salon
Restaurant (no alcohol sales; outdoor dining permitted by design review)¹
Retail Sales²
Schools, Public: (Elementary, Jr. High, High School)
Scientific Instruments Sales
Second Hand Dealer
Second Hand Store
Shoe Repair/Sales
Sidewalk Dining (requires encroachment permit)¹
Sign Shop
Silk Screen Printing Shop
Sporting Goods Sales
Stationery Store
Surf Shop Retail and Rental
Swimming Pool Supplies/Equipment Sales
Tailor Shop
Telephone Answering Service
Theaters and Places of Public Assembly
Tile Sales
Utility Lines - Sewer, Gas, Petroleum, Telephone, Electric

¹ See Section B.4.d Sidewalk Dining in this chapter for additional provisions.

² Retail sales uses consistent with the intent of the N-CM-2 Zone are permitted.

Veterinarian (without Kennel)
Video Sales & Rental
Wearing Apparel Shop
Wood Product Sales, Retail

2. Minor Use Permits:

Alcoholic Beverage, Sales - off premise
Auction House
Automobile Body Repair
Automobile Elec. & Tune-up
Automobile Painting
Automobile Rental
Automobile Repair Garage
Auto Sales, New and Used (Incidental repairs permitted per
State Certification Regulations)
Bed & Breakfast
Boat Rental
Boat Sales
Cabinet Shop
Club, Athletic or Recreational
Club, Private
Courts, Commercial (Badminton, Tennis, Racquetball, Others)
Day Care Center
Gas Engines, Retail Sales and Service
Gymnasium, Public Health Center
Horticultural Services
Laboratories (biochemical, film, other)
Laboratories (medical, dental)
Motorcycle Sales Only
Motorcycle Sales & Service
Muffler Shop
Nurseries, Horticultural
Outdoor Sales
Packaging Business
Parks and Recreation Areas
Pawnshop
Recording Studio
Recreational Facilities Private
Recreational Facilities Public
Recreational Vehicle Sales
Restaurant (w/ alcohol sales; outdoor dining permitted)¹
Small Animal Hospital/Kennel
Truck Rental
Truck Sales
Video Production Studio
Wholesale Business²

¹ See Section B.4.d Sidewalk Dining in this chapter for additional provisions.

² Wholesale businesses consistent with the intent of the N-CM-2 zone are permitted.

3. Major Use Permits:

Athletic Field
Automobile Gasoline sales/dispensing (including auto repair, service, or car wash)
Bar/Cocktail Lounge
Car Wash¹
Cellular Facility
Church/Temple/Religious Institution
Club, with Alcohol Sales
Educational Institution, Private
Electrical Distribution Substation
Emergency Residential Shelter
Garage, Public parking
Gas Distribution, Meter and Control Station
Hotel
Massage Parlor (see Adult Use 30.51, Title 30 of the Encinitas Municipal Code)
Medical Complex
Motel
Parking Lot, Commercial
Police/Sheriff Station
Radio/Television Broadcasting Studio
Radio/Television Transmitter
Schools, Private: (Elementary, Jr. High, High School)
Schools, Technical
Swap Meet
Time Share
Transitional Housing

¹ Permitted as an accessory use with the approval of a Major Use Permit only in association with an automobile gasoline sales/dispensing principal use.

4. Development Standards - Commercial or Mixed-Use Developments:

a. General Standards:

Net Lot Area:	6,000 net square feet minimum
Lot Width:	75 feet
Lot Depth:	80 feet
Front Yard Setback:	0 feet ¹
Side Yard Setback for each interior side:	0 feet ²
Side Yard Setback street side:	0 feet ²
Rear Yard Setback:	0 feet ^{2/3}
Lot Coverage (maximum):	90 percent
Building Height maximum:	33 feet or three stories, whichever is less.
Landscaping:	10 percent ⁴ (See Landscape Recommendations, Section 4.8)
Off-Street Parking:	See Parking Requirements, Section 3.3
Floor Area Ratio:	No requirement

b. Mixed-Use Site Planning:

1. Separate entrances shall be required for residential and commercial uses when occupying the same structure. (This does not preclude secondary interior access between commercial and residential uses, in addition to the required separate accesses, provided that residential access to or through commercial uses avoids unsafe commercial areas.)
2. Residential uses shall be located either above or behind the primary commercial use and shall not occupy the ground floor building frontage area along Highway 101.
3. All detached or attached dwelling units in mixed-use developments must be constructed on a legally subdivided lot or must be subdivided to permit ownership of airspace in the form of a dwelling unit with an undivided share in common elements.
4. Residential uses shall not exceed 50 percent of the gross building floor area for the development site, unless authorized through the approval of a Major Use Permit.
5. Residential densities shall be calculated pursuant to Section 30.16.010.B 1. and 2. of Title 30 of the Encinitas Municipal Code.

¹ Lots with frontage on Melrose Avenue or La Veta Avenue, which do not extend through to Highway 101, shall maintain a twenty (20) foot front yard setback from Melrose/La Veta.

² A minimum ten (10) foot rear or side yard setback shall be required where any portion of the rear or side yard immediately abuts a residentially zoned property.

³ Lots with frontage on North Highway 101 which extend through to Melrose Avenue or La Veta Avenue, shall maintain a minimum twenty (20) foot rear yard setback from Melrose Avenue or La Veta Avenue.

⁴ Pedestrian plazas or decorative landscaping, which include site amenities such as sculpture, fountains, planters, enhanced paving, etc., may be counted towards meeting the respective landscaping minimum, up to a maximum of 50 percent of the respective landscaping minimum.

c. Additional Setbacks and Separation from Adjacent Land Uses:

1. Where a commercial or mixed-use development abuts property in any residential zone, a masonry wall of no less than six (6) feet in height is required along the property line between the commercial/mixed use development and the residentially-zoned property, and may be increased to eight (8) feet to resolve any noise or visual impacts. Said wall shall be measured from the highest finished grade at the property line, and shall be erected and maintained as separation between the commercial and residential uses. Said wall shall be stepped down to no more than four (4) feet in height within 10 feet of the front property line.
2. For lots fronting along North Highway 101 which extend through to Melrose Avenue, a minimum landscape area of ten (10) foot in depth shall be required along Melrose Avenue. A masonry wall of no less than six (6) feet in height, which may be increased to eight (8) feet to resolve any noise or visual impacts, measured from the highest finished grade at the property line shall be erected along the landscape area, located a minimum of ten (10) feet from the property line.

d. Sidewalk Dining:

1. Outdoor/sidewalk dining areas that encroach into the public right-of-way shall require an encroachment permit from the Engineering Services Department and must maintain a minimum 4-foot clearance which excludes planter areas, fire hydrants, street lights, other street furniture, on-street auto parking overhang, and any approved A-frame signage.
2. Encroachment permits for outdoor/sidewalk dining within public rights-of-way require the following information and are subject to the following limitations:
 - a. Submit a brief description of the proposed encroachment and its proposed location in the public right-of-way.
 - b. Submit a site plan, in a form acceptable to the Engineering Services Department and the Community Development Department, showing local conditions, including street and sidewalk width, and location/dimensions of all street furniture and elements on the sidewalk.
 - c. Specify the number of tables and seating requested and not-to-exceed-amount.
 - d. An issued permit may be revoked by the City after a 30-day notice.
 - e. Hours of operation shall be pre-determined through the encroachment permit and limited to an associated adjacent eating and drinking establishment.
 - f. No sound amplification device, musical instrument or sound reproduction device shall be operated or used with outdoor dining, and outdoor lighting shall comply with performance standards, and be approved by the City.

- g. Prior to issuance and approval of a permit, a finding shall be made by the Community Development Director or designee that the outdoor dining will not adversely affect the neighborhood nor be detrimental to persons residing, visiting or working in the area.
- h. A liability insurance policy naming the city as additionally insured for \$1,000,000 shall be on file with the Engineering Services Department and the Community Development Department.

e. General Site Planning:

- 1. Site planning shall include consideration of adjoining parcels in terms of building configuration, building design, and scale of landscaping materials, circulation/parking configuration and access.
- 2. All commercial uses, including storage, shall be located within an enclosed building unless authorized by a Minor Use Permit, or as otherwise permitted by this Specific Plan.

f. Building Design and Use:

- 1. Visual relief shall be provided for linear buildings by the use of architectural projections or recessed areas which create the appearance of smaller adjoining buildings.
- 2. Energy consumption in buildings should be minimized using the following features where feasible:
 - Cogeneration
 - Solar access
 - South facing windows with eave coverage
 - Double glazed windows
 - Deciduous shade trees
 - Good ventilation
 - Efficient lighting
 - Day lighting
- 3. All signage associated with the buildings and site shall be integrated in terms of both architecture and color and shall comply with Section 4.5.6 of the Specific Plan.

g. Access, Circulation, and Parking:

- 1. Vehicular access shall be taken from an abutting alley or adjacent street rather than from North Highway 101 where feasible.
- 2. Where vehicular access off of North Highway 101 is necessary, access shall be limited to one point for each 300 feet of frontage or one point per parcel if frontage is less than 300 feet.
- 3. All alleys shall be maintained for vehicular access and/or pedestrian paths, and shall not be vacated.

4. Access points to adjoining lots shall be shared wherever feasible. Reciprocal ingress and egress, circulation, and parking arrangements shall be required to facilitate the ease of vehicular movement between adjoining properties where feasible;
5. Sidewalks shall be located along the west side of North Highway 101 and other street frontages per streetscape concepts and circulation improvement standards under this Specific Plan;
6. Where feasible, on-site parking areas shall be located off the alley or to the rear of a building. On-site parking areas abutting all local roads or North Highway 101 shall be screened by earth mounding, landscaping, low decorative walls, or a combination of any of these methods, three (3) feet in height.
7. For lots fronting along North Highway 101 which extend through to Melrose Avenue, vehicular access from Melrose Avenue shall be prohibited unless the location of existing structures to continue in use prohibits access from an abutting alley, side street or Highway 101. Where access from Melrose must be provided, access is limited to no more than one point for each 300 feet of frontage or one point per parcel where frontage is less than 300 feet.

h. Auxiliary Structures/Equipment and Utilities:

1. All roof appurtenances including, but not limited to, air conditioning units, and mechanical/electrical equipment shall be shielded and architecturally screened from view from on-site parking areas, adjacent public streets and adjacent properties.
2. All ground-mounted mechanical/electrical equipment, including heating and air conditioning units, and trash receptacle areas, shall be completely screened from surrounding properties by use of a wall, fence, or landscaping, or shall be enclosed within a building.
3. All utility connections shall be designed to coordinate with the architectural elements of the site so as not to be exposed except where necessary. Pad-mounted transformers and/or meter box locations shall be included in the site plan with an appropriate screening treatment. All new and existing utility connections within the boundaries of the project shall be placed underground, with the exception of existing overhead power transmission lines with voltage levels exempted by the Municipal Code and long distance and main trunk communications facilities. Transformer, terminal boxes, meter cabinets, pedestals, ducts and other facilities may be placed above ground provided they are screened.
4. Trash receptacles shall be located behind the building, where possible. In all locations, trash receptacles shall be enclosed by a six-foot high decorative masonry wall with view-obstructing gates; for lots with frontage extending through to Melrose Avenue, trash storage shall be behind the twenty (20) foot setback required from Melrose Avenue.

5. Outdoor storage areas, when approved by minor use permit, shall be located to the rear of a building and shall be enclosed by solid decorative masonry walls and view-obstructing gates, both to be not less than six (6) feet in height to adequately screen such areas from view. Reasonable substitutions such as masonry, wood or metal pilasters with wrought iron or vinyl-clad chain link and view obscuring material may be approved through Design Review.
6. For lots fronting along North Highway 101 which extend through to Melrose Avenue, all auxiliary structures/equipment, and utilities shall be setback a minimum of twenty (20) feet from Melrose Avenue.

i. Landscaping:

1. Site landscaping shall include a combination of trees, shrubbery, vines, or groundcovers all of which shall be drought-tolerant, where feasible. Landscaping in parking areas shall be predominantly trees to provide shade and visual relief.
2. Where feasible, landscaping in parking areas shall include one fifteen (15) gallon tree for every four (4) parking spaces distributed throughout the parking area.
3. All landscaped areas shall be watered by an automatic irrigation system with underground piping (excluding drip or soaker irrigation systems) and regularly maintained in a healthy and thriving condition free of weeds, trash, debris.
4. A minimum of 10 percent of the site area shall be landscaped. Street trees shall be provided in accordance with the specific plan landscape recommendations. Pedestrian plazas or decorative landscaping, which include site amenities such as sculpture, fountains, planters, enhanced paving, etc., may be counted towards meeting the respective landscaping minimum, up to a maximum of 50 percent of the respective landscaping minimum.
5. Landscaping shall be in accordance with Section 4.8, Landscape Recommendations.

j. Lighting:

1. Lighting placed upon the building shall be architecturally integrated.
2. All light sources shall be shielded in such a manner that the light is directed away from streets or adjoining properties. Illuminators should be integrated within the architecture of the building. Freestanding lamp posts shall be no taller than eighteen (18) feet. The intensity of light at the boundary of the N-CM-2 Zone shall not exceed seventy-five (75) foot lamberts from the source of light.

k. Architectural Features, Accessory Structures and Walls/Fences:

1. Accessory structures shall not be located in front of or on the street side of the main building(s).
2. Accessory structures shall meet the setback requirements for main building(s).
3. Between any street property line and a principal structure on that property, no wall or fence shall exceed four (4) feet in height. Where a retaining wall is used to increase usable lot area, the height of said retaining wall between any street property line and the principal structure shall not exceed four (4) feet in height.
4. The use of chain link, barbed wire and razor wire fencing within public view shall be prohibited.

l. Condominium Conversion

The conversion of a project to condominium ownership shall meet all requirements of the zone to the maximum extent possible within the constraints of the existing development. In no case shall the requirements for the screening of storage/sales areas or mechanical equipment be waived.

m. Design Recommendations

Applicable provisions of the Specific Plan Design Recommendations, Chapter 4 of this Specific Plan, shall apply to proposed development under this zone. If a conflict arises between provisions of the design recommendations and the development standards of this zone, the development standards shall take precedence.

n. Signage Regulations

Except as otherwise specified in this Section and Chapter 4 of this Specific Plan, the signage standards, and review and approval procedures of Chapters 23.08 and 30.60 of the Encinitas Municipal Code shall apply for all signs within the North 101 Corridor Specific Plan.

o. Freestanding A-frame signage

In addition to other types of permitted signage, freestanding A-frame signs shall be allowed, subject to the following standards:

1. one (1) per business.
2. maximum five (5) feet in height by three (3) feet in width.
3. maximum of two (2) faces per sign, back-to-back.
4. no external or internal illumination.
5. not permanently affixed to any object, structure or the ground, including utility poles, light poles, trees or any merchandise or products displayed outside of buildings.
6. portable, self-supporting, stable and weighted or constructed to withstand overturning by wind or contact.
7. not on display during non-business hours.
8. located directly in front of the building/business it is advertising.
9. not placed in such a way as to interfere with pedestrian or vehicular sight lines or corner clear zone requirements as specified by the City.
10. not placed in such a way as to obstruct access to a public sidewalk, public street, driveway, parking space, fire door, fire escape, handicapped access or in such a way that it obstructs free passage over any public right-of-way.
11. not obscure or interfere with the effectiveness of any official notice or public safety device.
12. maintain at least a four (4) foot width for pedestrian clearance over the entire length of the sidewalk or the public right-of-way in front of the business, free of all obstructions such as planter areas, fire hydrants, street lights, other street furniture, on-street parking overhang, and any other approved public right-of-way dining area.

p. Placement of A-frame Signs in the Public Right-of-Way

In addition to meeting the above standards, A-frame signs which encroach into the public right-of-way are allowed with the issuance of an encroachment permit from the Engineering Services Department. Encroachment permits for A-frame signs in the public right-of-way shall require the submittal of the following information and are subject to the following limitations:

1. Submit a brief description of the proposed encroachment and its proposed location in the public right-of-way.
2. Submit a site plan, in a form acceptable to the Engineering Services Department and the Community Development Department, showing local conditions, including street and sidewalk width, and location/dimensions of all street furniture and elements on the sidewalk.
3. The description and site plan shall be reviewed and accepted by the Community Development Department prior to the issuance of an encroachment permit by the Engineering Services Department.
4. A liability insurance policy naming the City as additionally insured in the amount of \$1,000,000 shall be on file with the Engineering Services Department and the Community Development Department.
5. An issued encroachment permit may be revoked by the City after a thirty (30) day notice.

C. Zone: Commercial Mixed 3 (N-CM-3)

This zone is intended to provide local neighborhood-serving and visitor-serving commercial uses, excluding auto-related uses, in the general area along Second Street directly north of B Street, and generally between North Highway 101 and Third Street. The area serves as a gateway into the North 101 Corridor Specific Plan area from the south, as well as a commercial node to serve residents and visitors utilizing nearby recreational facilities such as Moonlight State Beach and Park. This zone allows 1) stand-alone commercial; or 2) mixed use, with commercial use types to be generally neighborhood-serving and visitor-serving as per the permitted, minor use permit, and major use permit principal use list of this section.

Commercial allows a wide range of general commercial uses which generally serve the needs of local, neighborhood residents and visitors to the area, and which are compatible with the surrounding residential neighborhoods.

Mixed Use allows commercial and residential uses at a maximum density of 25.0 dwelling units per net acre on the same property or in the same structure, with the intent of providing opportunities for housing and live/work or artisan loft arrangements, while also mitigating the impacts between commercial and residential uses.

It is the intention of the N-CM-3 Zone to allow for functional and physical integration of project components of mixed uses. Consideration will be given to joint use of parking, sizes of residential units and overall architectural design when considering approvals for mixed use developments.

The following principal uses shall be permitted in the N-CM-3 Zone and are identified as either permitted, or requiring a minor use permit or major use permit. All other principal uses not specifically listed are prohibited.

1. Permitted Uses:

- Accessory Building
- Alcoholic Beverage, Sales - off premise
- Antique Sales, Retail
- Aquarium, Commercial
- Arcade Accessory (Non-Adult)
- Art Gallery
- Artisan/Craftsman (Sales & Production Studio)
- Bakery (Retail)
- Barber & Beauty Shop (Cosmetologist)
- Bicycle Sales, Rental and Service
- Billiard, Pool Hall
- Book Sales
- Bowling Alley
- Building Materials Sales Enclosed (10,000 sq. ft. or less)¹
- Candle Shop
- Candy and Confectionery Sales

¹ Building Materials Sales Enclosed uses less than 10,000 square feet in size are permitted. Building Materials Sales Enclosed uses greater than 10,000 square feet, but less than 25,000 square feet in size are allowed with the approval of a Major Use Permit.

Catering Service
Charitable Bins and Depositories (accessory)
Clothing Rental
Coins, Purchase and Sales
Computer Sales and Service
Conservatory of Music
Cosmetic Design Studio
Costume Rentals
Dance Hall
Dance Studio
Delicatessen
Dressmaking Shop
Drug Store
Dry Cleaning, Retail and Self-Service
Dwelling Unit, Caretaker - accessory to the principal use
Dwelling Unit, One-Family¹
Dwelling Unit, Two-Family¹
Dwelling Unit, Three-Family¹
Dwelling Unit, Multiple Family¹
Educational Institution, Public
Family Day Care Home, Small (Six or fewer persons, accessory to a dwelling only)
Family Day Care Home, Large (Seven to twelve persons, accessory to dwelling only)
Fire Station
Floor Covering, Retail
Florist Shop
Gem/Precious Stone, Retail
Gift Shop
Glass and Mirrors, Retail
Glass Edging and Beveling
Grocery Store
Group Home¹
Hair Salon
Hardware Store (10,000 sq. ft or less)²
Hat Shop
Health Food Store
Hobby Supply Shop
Home Occupations
Ice Cream Parlor
Interior Decorating Service
Jewelry Sales
Lapidary Shop, Retail (Gems)
Laundromat
Library
Locksmith
Market (food)
Medical/Dental Clinic
Medical/Dental Office
Music/Record Store
Museum
Newsstand
Notions and Dry Goods Store
Office (Business and Professional)

¹ Allowed in mixed-use developments only. All detached or attached dwelling units in mixed-use developments must be constructed on a legally subdivided lot or must be subdivided to permit ownership of airspace in the form of a dwelling unit with an undivided share in common elements.

² Hardware Store uses less than 10,000 square feet in size are permitted. Hardware Store uses greater than 10,000 square feet, but less than 25,000 square feet in size are allowed with the approval of a Major Use Permit.

3.0 Use and Development Regulations

Optical Products Sales
Pet Shop/Pet Grooming (grooming only, no kennels or boarding)
Pharmacy
Photocopy Shop
Photofinishing, Retail
Photographic Studio
Photographic Supplies
Photograving and Finishing
Post Office
Postal Annex, Private Ownership
Pottery Sales
Radio/Television Sales
Real Estate Office
Reducing Salon
Rental Recreational Surf Shop
Restaurant (no alcohol sales; outdoor dining permitted by design review)¹
Retail Sales¹
Schools, Public: (Elementary, Jr. High, High School)
Second Hand Dealer
Second Hand Store
Shoe Repair/Sales
Sidewalk Dining (requires an encroachment permit)²
Silk Screen Printing Shop
Sporting Goods Sales
Stationery Store
Surf Shop Retail and Rental (no manufacturing)
Tailor Shop
Theaters and Places of Public Assembly
Tile Sales, Custom
Utility Lines - Sewer, Gas, Petroleum, Telephone, Electric
Veterinarian (No Kennel)
Video Sales & Rental
Wearing Apparel Shop

2. Minor Use Permits

Auction House
Bed and Breakfast
Club, Athletic or Recreational
Club, Private
Courts, Commercial (Badminton, Tennis, Racquetball, Others)
Day Care Center
Gymnasium, Public Health Club
Nurseries, Horticultural
Outdoor Sales
Packaging Business
Parks and Recreational Areas
Recording Studio
Recreational Facilities Private
Recreational Facilities Public
Restaurant with alcohol sales (outdoor dining permitted by design review)²
Video Production Studio
Wholesale Business³

¹ Local neighborhood-serving and visitor-serving retail sales uses consistent with the intent of the N-CM-3 Zone are permitted.

² See Section C.4.d Sidewalk Dining in this chapter for additional provisions.

³ Local neighborhood-serving and visitor-serving wholesale businesses consistent with the intent of the N-CM-3 Zone are permitted.

3. Major Use Permits

Automobile gasoline sales/Dispensing (not including auto repair service)
Bar/Cocktail Lounge
Building Materials Sales Enclosed (greater than 10,000 sq. ft., less than 25,000 sq. ft.)¹
Car Wash²
Cellular Facility
Church/Temple/Religious Institution
Club, with alcohol sales
Convenience Store
Drive-Through Restaurant
Electrical Distribution Substation
Garage, Public Parking
Gas Distribution, Meter and Control Station
Hardware Store (greater than 10,000 sq. ft., less than 25,000 sq. ft.)³
Hotel
Massage Parlor
Medical Complex
Motel
Parking Lot, Commercial
Police/Sheriff Station
Radio/Television Broadcasting Studio
Radio/Television Transmitter
Timeshare
Transitional Housing

¹ Building Materials Sales Enclosed uses greater than 25,000 square feet in size are not allowed.

² Permitted as an accessory use with the approval of a Major Use Permit only in association with an automobile gasoline sales/dispensing principal use.

³ Hardware Store uses greater than 25,000 square feet in size are not allowed.

4. Development Standards - Commercial or Mixed-Use Developments:

a. General Standards:

Net Lot Area:	3,950 net square feet minimum
Lot Width:	40 feet
Lot Depth:	90 feet
Front Yard Setback:	0 feet
Side Yard Setback for each interior side:	0 feet
Side Yard Setback street side:	0 feet
Rear Yard Setback:	10 feet ¹
Lot Coverage (maximum):	90 percent
Building Height maximum:	33 feet or three stories, whichever is less.
Landscaping:	10/15 percent ² (See Landscape Recommendations, Section 4.8)
Off-Street Parking:	See Parking Requirements, Section 3.3
Floor Area Ratio:	No requirement

b. Mixed-Use Site Planning:

1. Separate entrances shall be required for residential and commercial uses when occupying the same structure. (This does not preclude secondary interior access between commercial and residential uses, in addition to the required separate accesses, provided that residential access to or through commercial uses avoids unsafe commercial areas.)
2. Residential uses shall be located either above or behind the primary commercial use and shall not occupy the ground floor building frontage area along Highway 101.
3. All detached or attached dwelling units in mixed-use developments must be constructed on a legally subdivided lot or must be subdivided to permit ownership of airspace in the form of a dwelling unit with an undivided share in common elements.
4. Residential uses shall not exceed 50 percent of the gross building floor area for the development site, unless authorized through the approval of a Major Use Permit.
5. Residential densities shall be calculated pursuant to Section 30.16.010.B 1. and 2. of Title 30 of the Encinitas Municipal Code.

¹ A minimum ten (10) foot rear or side yard setback shall be required where any portion of the rear or side yard immediately abuts a residentially zoned property.

² All legal lots 10,000 square feet or less in size shall be required to provide a minimum of 10 percent landscaping. All legal lots exceeding 10,000 square feet in size shall be required to provide a minimum of 15 percent landscaping. Pedestrian plazas or decorative landscaping, which include site amenities such as sculpture, fountains, planters, enhanced paving, etc., may be counted towards meeting the respective landscaping minimum, up to a maximum of 50 percent of the respective landscaping minimum.

c. Additional Setbacks and Separation from Adjacent Land Uses:

Where a commercial or mixed-use development abuts property in any residential zone, a masonry wall of no less than six (6) feet in height is required along the property line between the commercial/mixed use development and the residentially-zoned property, and may be increased to eight (8) feet to resolve any noise or visual impacts. Said wall shall be measured from the highest finished grade at the property line, and shall be erected and maintained as separation between the commercial and residential uses. Said wall shall be stepped down to no more than four (4) feet in height within 10 feet of the front property line.

d. Sidewalk Dining:

1. Outdoor/sidewalk dining areas that encroach into the public right-of-way shall require an encroachment permit from the Engineering Services Department and must maintain a minimum 4-foot clearance which excludes planter areas, fire hydrants, street lights, other street furniture, on-street auto parking overhang, and any approved A-frame signage.
2. Encroachment permits for outdoor/sidewalk dining within public rights-of-way require the following information and are subject to the following limitations:
 - a. Submit a brief description of the proposed encroachment and its proposed location in the public right-of-way.
 - b. Submit a site plan, in a form acceptable to the Engineering Services Department and the Community Development Department, showing local conditions, including street and sidewalk width, and location/dimensions of all street furniture and elements on the sidewalk.
 - c. Specify the number of tables and seating requested and not-to-exceed-amount.
 - d. An issued permit may be revoked by the City after a 30-day notice.
 - e. Hours of operation shall be pre-determined through the encroachment permit and limited to an associated adjacent eating and drinking establishment.
 - f. No sound amplification device, musical instrument or sound reproduction device shall be operated or used with outdoor dining, and outdoor lighting shall comply with performance standards, and be approved by the City.
 - g. Prior to issuance and approval of a permit, a finding shall be made by the Community Development Director or designee that the outdoor dining will not adversely affect the neighborhood nor be detrimental to persons residing, visiting or working in the area.
 - h. A liability insurance policy naming the city as additionally insured for \$1,000,000 shall be on file with the Engineering Services Department and the Community Development Department.

e. General Site Planning:

1. Site planning shall include consideration of adjoining parcels in terms of building configuration, building design, and scale of landscaping materials, circulation/parking configuration and access.
2. All commercial uses, including storage, shall be located within an enclosed building unless authorized by a Minor Use Permit, or as otherwise permitted by this Specific Plan.

f. Building Design and Use:

1. Visual relief shall be provided for linear buildings by the use of architectural projections or recessed areas which create the appearance of smaller adjoining buildings.
2. Energy consumption in buildings should be minimized using the following features where feasible:
 - Cogeneration
 - Solar access
 - South facing windows with eave coverage
 - Double glazed windows
 - Deciduous shade trees
 - Good ventilation
 - Efficient lighting
 - Day lighting
3. All signage associated with the buildings and site shall be integrated in terms of both architecture and color and shall comply with Section 4.5.6 of the specific plan.

g. Access, Circulation, and Parking:

1. Vehicular access shall be taken from an abutting alley or adjacent street rather than from North Highway 101 where feasible.
2. Where vehicular access off of North Highway 101 is necessary, access shall be limited to one point for each 300 feet of frontage or one point per parcel if frontage is less than 300 feet.
3. All alleys shall be maintained for vehicular access and/or pedestrian paths, and shall not be vacated.
4. Access points to adjoining lots shall be shared wherever feasible. Reciprocal ingress and egress, circulation, and parking arrangements shall be required to facilitate the ease of vehicular movement between adjoining properties where feasible;
5. Sidewalks shall be located along the west side of North Highway 101 and other street frontages per streetscape concepts and circulation improvement standards under this Specific Plan;

6. Where feasible, on-site parking areas shall be located off the alley or to the rear of a building. On-site parking areas abutting all local roads or North Highway 101 shall be screened by earth mounding, landscaping, low decorative walls, or a combination of any of these methods, three (3) feet in height.

h. Auxiliary Structures/Equipment and Utilities:

1. All roof appurtenances including, but not limited to, air conditioning units, and mechanical/electrical equipment shall be shielded and architecturally screened from view from on-site parking areas, adjacent public streets and adjacent properties.
2. All ground-mounted mechanical/electrical equipment, including heating and air conditioning units, and trash receptacle areas, shall be completely screened from surrounding properties by use of a wall, fence, or landscaping, or shall be enclosed within a building.
3. All utility connections shall be designed to coordinate with the architectural elements of the site so as not to be exposed except where necessary. Pad-mounted transformers and/or meter box locations shall be included in the site plan with an appropriate screening treatment. All new and existing utility connections within the boundaries of the project shall be placed underground, with the exception of existing overhead power transmission lines with voltage levels exempted by the Municipal Code and long distance and main trunk communications facilities. Transformer, terminal boxes, meter cabinets, pedestals, ducts and other facilities may be placed above ground provided they are screened.
4. Trash receptacles shall be located behind the building, where possible. In all locations, trash receptacles shall be enclosed by a six-foot high decorative masonry wall with view-obstructing gates.
5. Outdoor storage areas, when approved by minor use permit, shall be located to the rear of a building and shall be enclosed by solid decorative masonry walls and view-obstructing gates, both to be not less than six (6) feet in height to adequately screen such areas from view. Reasonable substitutions such as masonry, wood or metal pilasters with wrought iron or vinyl-clad chain link and view obscuring material may be approved through Design Review.

i. Landscaping:

1. Site landscaping shall include a combination of trees, shrubbery, vines, or groundcovers all of which shall be drought-tolerant, where feasible. Landscaping in parking areas shall be predominantly trees to provide shade and visual relief.
2. Where feasible, landscaping in parking areas shall include one fifteen (15) gallon tree for every four (4) parking spaces distributed throughout the parking area.

3. All landscaped areas shall be watered by an automatic irrigation system with underground piping (excluding drip or soaker irrigation systems) and regularly maintained in a healthy and thriving condition free of weeds, trash, debris.
4. All legal lots 10,000 square feet or less in size shall be required to provide a minimum of 10 percent landscaping. All legal lots exceeding 10,000 square feet in size shall be required to provide a minimum of 15 percent landscaping. Street trees shall be provided in accordance with the specific plan landscape recommendations. Pedestrian plazas or decorative landscaping, which include site amenities such as sculpture, fountains, planters, enhanced paving, etc., may be counted towards meeting the respective landscaping minimum, up to a maximum of 50 percent of the respective landscaping minimum.
5. Landscaping shall be in accordance with Section 4.8, Landscape Recommendations.

j. Lighting:

1. Lighting placed upon the building shall be architecturally integrated.
2. All light sources shall be shielded in such a manner that the light is directed away from streets or adjoining properties. Illuminators should be integrated within the architecture of the building. Freestanding lamp posts shall be no taller than eighteen (18) feet. The intensity of light at the boundary of the N-CM-3 Zone shall not exceed seventy-five (75) foot lamberts from the source of light.

k. Architectural Features, Accessory Structures and Walls/Fences:

1. Accessory structures shall not be located in front of or on the street side of the main building(s).
2. Accessory structures shall meet the setback requirements for main building(s).
3. Between any street property line and a principal structure on that property, a wall or fence shall not exceed four (4) feet in height. Where a retaining wall is used to increase usable lot the height of said retaining wall, if located between any street property line and a principal structure, shall not exceed four (4) feet.
4. The use of chain link, barbed wire and razor wire fencing within public view shall be prohibited.

l. Condominium Conversion

The conversion of a project to condominium ownership shall meet all requirements of the zone to the maximum extent possible within the constraints of the existing development. In no case shall the requirements for the screening of storage/sales areas or mechanical equipment be waived.

m. Design Recommendations

Applicable provisions of the Specific Plan Design Recommendations, Chapter 4 of this Specific Plan, shall apply to proposed development under this zone. If a conflict arises between provisions of the design recommendations and the development standards of this zone, the development standards shall take precedence.

n. Signage Regulations

Except as otherwise specified in this Section and Chapter 4 of this Specific Plan, the signage standards, and review and approval procedures of Chapters 23.08 and 30.60 of the Encinitas Municipal Code shall apply for all signs within the North 101 Corridor Specific Plan.

o. Freestanding A-frame signage

In addition to other types of permitted signage, freestanding A-frame signs shall be allowed, subject to the following standards:

1. one (1) per business.
2. maximum five (5) feet in height by three (3) feet in width.
3. maximum of two (2) faces per sign, back-to-back.
4. no external or internal illumination.
5. not permanently affixed to any object, structure or the ground, including utility poles, light poles, trees or any merchandise or products displayed outside of buildings.
6. portable, self-supporting, stable and weighted or constructed to withstand overturning by wind or contact.
7. not on display during non-business hours.
8. located directly in front of the building/business it is advertising.
9. not placed in such a way as to interfere with pedestrian or vehicular sight lines or corner clear zone requirements as specified by the City.
10. not placed in such a way as to obstruct access to a public sidewalk, public street, driveway, parking space, fire door, fire escape, handicapped access or in such a way that it obstructs free passage over any public right-of-way.
11. not obscure or interfere with the effectiveness of any official notice or public safety device.

12. maintain at least a four (4) foot width for pedestrian clearance over the entire length of the sidewalk or the public right-of-way in front of the business, free of all obstructions such as planter areas, fire hydrants, street lights, other street furniture, on-street parking overhang, and any other approved public right-of-way dining area.

p. Placement of A-frame Signs in the Public Right-of-Way

In addition to meeting the above standards, A-frame signs which encroach into the public right-of-way are allowed with the issuance of an encroachment permit from the Engineering Services Department. Encroachment permits for A-frame signs in the public right-of-way shall require the submittal of the following information and are subject to the following limitations:

1. Submit a brief description of the proposed encroachment and its proposed location in the public right-of-way.
2. Submit a site plan, in a form acceptable to the Engineering Services Department and the Community Development Department, showing local conditions, including street and sidewalk width, and location/dimensions of all street furniture and elements on the sidewalk.
3. The description and site plan shall be reviewed and accepted by the Community Development Department prior to the issuance of an encroachment permit by the Engineering Services.
4. A liability insurance policy naming the City as additionally insured in the amount of \$1,000,000 shall be on file with the Engineering Services Department and the Community Development Department.
5. An issued encroachment permit may be revoked by the City after a thirty (30) day notice.

D. Zone: Commercial Residential Mixed 1 (N-CRM-1)

This zone is intended to provide a zoning district where the development of general commercial uses, mixed use or stand-alone residential uses may be allowed. The zone provides for a variety of development opportunities including 1) stand-alone commercial (excluding auto-related uses), 2) mixed use, or 3) stand alone residential. The N-CRM-1 zone applies to properties located along North Highway 101 from and including the "Handy Rentals" property (Assessor Parcel Number 216-041-06) located immediately north of the emergency access to the Sea Bluff Condominiums, to West Glaucus Street. The intent of this zone is to achieve an eclectic blend of uses and architectural styles by orienting buildings to North Highway 101, emphasizing a pedestrian scale, and continuing to emphasize the use of dominating trees in the streetscape.

Commercial allows a wide range of general commercial activities, including retailing, service, and visitor-serving uses with the intent of accommodating citywide or regional needs and serving the needs of persons visiting the city for business and recreational purposes.

Mixed Use allows commercial and residential uses at a maximum density of 25.0 dwelling units per net acre on the same property or in the same structure, with the intent of providing opportunities for housing and live/work or artisan loft arrangements, while also mitigating the impacts between commercial and residential uses.

It is the intention of the N-CRM-1 Zone to allow for functional and physical integration of project components of mixed uses. Consideration will be given to joint use of parking, sizes of residential units and overall architectural design when considering approvals for mixed use developments.

Residential allows single-family detached or multi-family detached and attached residential units, at a maximum density of 25.0 dwelling units per net acre.

The following principal uses shall be permitted in the N-CRM-1 Zone and are identified as either permitted, or requiring a minor use permit or major use permit. All other principal uses not specifically listed are prohibited.

1. Permitted Uses:

- Accessory Building
- Antique Sales, Retail
- Appliance Sales & Repairs (Household and Small Appliances)
- Aquarium, Commercial
- Arcade Accessory (Non-Adult)
- Art Gallery
- Artisan/Craftsman (Sales & Studio)
- Bakery (Retail)
- Bank/Savings and Loan
- Barber and Beauty Shop (Cosmetologist)
- Bicycle Sales, Rental and Service
- Billiard, Pool Hall
- Blueprinting and Photostatting
- Book Sales
- Bookbinding
- Bowling Alley

3.0 Use and Development Regulations

Building Materials Sales, Enclosed
Candle Shop
Candy and Confectionery Sales
Catering Service
Charitable Bins and Depositories (accessory)
Clothing Rental
Coins, Purchase and Sales
Computer Sales & Service
Conservatory of Music
Convenience Store
Cosmetic Design Studio
Costume Rentals
Dairy Store
Dance Hall
Dance Studio
Delicatessen
Dental Clinic
Dressmaking Shop
Drug Store
Dry Cleaning, Retail and Self-Service
Dwelling Unit, Caretaker -accessory to the principal
Dwelling Unit, One-Family¹
Dwelling Unit, Two-Family¹
Dwelling Unit, Three-Family¹
Dwelling Unit, Multiple Family¹
Educational Institution, Public
Employment Agency
Family Day Care Home, Small (Six or fewer persons, accessory to a dwelling only)
Family Day Care Home, Large (Seven to twelve persons, accessory to dwelling only)
Finance Company
Fire Prevention Equipment, Sales and Service
Fire Station
Floor Covering, Retail
Florist Shop
Furniture Sales
Garden Supplies
Gem/Precious Stone Retail
Gift Shop
Glass and Mirrors, Retail
Glass Edging and Beveling
Glass Studio (Stained and others)
Grocery Store
Group Home
Hair Salon
Hardware Store
Hat Shop
Health Food Store
Hobby Supply Shop
Home & Business Maintenance Service
Home Occupations
Ice Cream Parlor
Interior Decorating Service
Janitorial Services
Janitorial Supplies/Sales
Jewelry Sales

¹ All detached or attached dwelling units in residential-only or mixed-use developments must be constructed on a legally subdivided lot or must be subdivided to permit ownership of airspace in the form of a dwelling unit with an undivided share in common elements.

Lapidary Shop, Retail (Gems)
Laundromat
Laundry
Lawnmower Sales and Service
Library
Locksmith
Machine and Tool Sales
Mail Order House
Market (food)
Medical/Dental Clinic
Medical/Dental Office
Medical Equipment Sales
Museum
Music/Record Store
Newsstand
Notions and Dry Goods Store
Office (Business and Professional)
Office Equipment/Supplies Sales
Optical Products Sales
Orthopedic Devices Sales
Paper Product Sales
Parcel Delivery Service, Office only
Pet Shop/Pet Grooming
Pharmacy
Photocopy Shop
Photofinishing, Retail
Photographic Studio
Photographic Supplies
Photograving and Finishing
Post Office
Postal Annex, Private Ownership
Pottery Sales
Printing
Public Utilities: Office
Radio/Television Sales
Real Estate Office
Reducing Salon
Restaurant (no alcohol sales; outdoor dining permitted by design review)¹
Retail Sales²
Schools, Public: (Elementary, Jr. High, High School)
Scientific Instruments Sales
Second Hand Dealer
Second Hand Store
Shoe Repair/Sales
Sidewalk Dining (requires encroachment permit)¹
Sign Shop
Silk Screen Printing Shop
Sporting Goods Sales
Stationery Store
Surf Shop Retail and Rental
Swimming Pool Supplies/Equipment Sales
Tailor Shop
Telephone Answering Service
Theaters and Places of Public Assembly
Tile Sales
Utility Lines - Sewer, Gas, Petroleum, Telephone, Electric

¹ See Section D.4.d Sidewalk Dining in this chapter for additional provisions.

² Retail sales uses consistent with the intent of the N-CRM-1 Zone are permitted.

Veterinarian (No Kennel)
Video Sales & Rental
Wearing Apparel Shop
Wood Product Sales, Retail

2. Minor Use Permits:

Alcoholic Beverage, Sales - off premise
Auction House
Bed & Breakfast
Cabinet Shop
Club, Athletic or Recreational
Club, Private
Courts, Commercial (Badminton, Tennis, Racquetball, Others)
Day Care Center
Gymnasium, Public Health Center
Horticultural Services
Laboratories (biochemical, film, other)
Laboratories (medical, dental)
Nurseries, Horticultural
Outdoor Sales
Packaging Business
Parks and Recreation Areas
Pawnshop
Recording Studio
Recreational Facilities Private
Recreational Facilities Public
Restaurant (w/alcohol sales; outdoor dining permitted)¹
Small Animal Hospital/Kennel
Video Production Studio
Wholesale Business²

3. Major Use Permits:

Athletic Field
Bar/Cocktail Lounge
Cellular Facility
Church/Temple/Religious Institution
Club, with Alcohol Sales
Educational Institution, Private
Electrical Distribution Substation
Emergency Residential Shelter
Garage, Public parking
Gas Distribution, Meter and Control Station
Hotel
Massage Parlor (see Adult Use 30.51, Title 30 of the Encinitas Municipal Code)
Medical Complex
Motel
Parking Lot, Commercial
Police/Sheriff Station
Radio/Television Broadcasting Studio
Radio/Television Transmitter
Schools, Private: (Elementary, Jr. High, High School)
Schools, Technical
Swap Meet
Time Share
Transitional Housing

¹ See Section D.4.d Sidewalk Dining in this chapter for additional provisions.

² Wholesale businesses consistent with the intent of the N-CRM-1 zone are permitted.

4. Development Standards - Commercial or Mixed-Use Developments:

a. General Standards:

Net Lot Area:	6,000 net square feet minimum
Lot Width:	75 feet
Lot Depth:	80 feet
Front Yard Setback:	0 feet
Side Yard Setback for each interior side:	0 feet ¹
Side Yard Setback street side:	0 feet ¹
Rear Yard Setback:	0 feet ¹
Lot Coverage (maximum):	90 percent
Building Height maximum:	33 feet or three stories, whichever is less.
Landscaping:	10 percent ² (See Landscape Recommendations, Section 4.8)
Off-Street Parking:	See Parking Requirements, Section 3.3
Floor Area Ratio:	No requirement

b. Mixed-Use Site Planning:

1. Separate entrances shall be required for residential and commercial uses when occupying the same structure. (This does not preclude secondary interior access between commercial and residential uses, in addition to the required separate accesses, provided that residential access to or through commercial uses avoids unsafe commercial areas.)
2. Residential uses shall be located either above or behind the primary commercial use and shall not occupy the ground floor building frontage area along Highway 101.
3. All detached or attached dwelling units in mixed-use developments must be constructed on a legally subdivided lot or must be subdivided to permit ownership of airspace in the form of a dwelling unit with an undivided share in common elements.
4. Residential uses shall not exceed 50 percent of the gross building floor area for the development site, unless authorized through the approval of a Major Use Permit.
5. Residential densities shall be calculated pursuant to Section 30.16.010.B 1. and 2. of Title 30 of the Encinitas Municipal Code.

¹ A minimum ten (10) foot rear or side yard setback shall be required where any portion of the rear or side yard immediately abuts a residentially zoned property.

² Pedestrian plazas or decorative landscaping, which include site amenities such as sculpture, fountains, planters, enhanced paving, etc., may be counted towards meeting the respective landscaping minimum, up to a maximum of 50 percent of the respective landscaping minimum.

c. Additional Setbacks and Separation from Adjacent Land Uses:

Where a commercial or mixed-use development abuts property in any residential zone, a masonry wall of no less than six (6) feet in height is required along the property line between the commercial/mixed use development and the residentially-zoned property, and may be increased to eight (8) feet to resolve any noise or visual impacts. Said wall shall be measured from the highest finished grade at the property line, and shall be erected and maintained as separation between the commercial and residential uses. Said wall shall be stepped down to no more than four (4) feet in height within 10 feet of the front property line.

d. Sidewalk Dining:

1. Outdoor/sidewalk dining areas that encroach into the public right-of-way shall require an encroachment permit from the Engineering Services Department and must maintain a minimum 4-foot clearance which excludes planter areas, fire hydrants, street lights, other street furniture, on-street auto parking overhang, and any approved A-frame signage.
2. Encroachment permits for outdoor/sidewalk dining within public rights-of-way require the following information and are subject to the following limitations:
 - a. Submit a brief description of the proposed encroachment and its proposed location in the public right-of-way.
 - b. Submit a site plan, in a form acceptable to the Engineering Services Department and the Community Development Department, showing local conditions, including street and sidewalk width, and location/dimensions of all street furniture and elements on the sidewalk.
 - c. Specify the number of tables and seating requested and not-to-exceed-amount.
 - d. An issued permit may be revoked by the City after a 30-day notice.
 - e. Hours of operation shall be pre-determined through the encroachment permit and limited to an associated adjacent eating and drinking establishment.
 - f. No sound amplification device, musical instrument or sound reproduction device shall be operated or used with outdoor dining, and outdoor lighting shall comply with performance standards, and be approved by the City.
 - g. Prior to issuance and approval of a permit, a finding shall be made by the Community Development Director or designee that the outdoor dining will not adversely affect the neighborhood nor be detrimental to persons residing, visiting or working in the area.
 - h. A liability insurance policy naming the city as additionally insured for \$1,000,000 shall be on file with the Engineering Services Department and the Community Development Department.

5. Development Standards - Residential-Only Developments:

- a. In residential-only developments, except as otherwise specified herein, standards pursuant to Chapter 30.16, R-25 Zone of Title 30 of the Encinitas Municipal Code shall apply.
- b. Maximum building height is thirty-three (33) feet or three (3) stories, whichever is less.
- c. All detached or attached dwelling units in residential-only developments must be constructed on a legally subdivided lot or must be subdivided to permit ownership of airspace in the form of a dwelling unit with an undivided share in common elements.

6. General Development Standards - Commercial, Mixed-Use, or Residential-Only Developments:

a. General Site Planning

1. Site planning shall include consideration of adjoining parcels in terms of building configuration, building design, and scale of landscaping materials, circulation/parking configuration and access.
2. All commercial uses, including storage, shall be located within an enclosed building unless authorized by a Minor Use Permit or as otherwise permitted by this Specific Plan.

b. Building Design and Use:

1. Visual relief shall be provided for linear buildings by the use of architectural projections or recessed areas which create the appearance of smaller adjoining buildings.
2. Energy consumption in buildings should be minimized using the following features where feasible:
 - Cogeneration
 - Solar access
 - South facing windows with eave coverage
 - Double glazed windows
 - Deciduous shade trees
 - Good ventilation
 - Efficient lighting
 - Day lighting
3. All signage associated with the buildings and site shall be integrated in terms of both architecture and color and shall comply with Section 4.5.6 of the specific plan.

c. Access, Circulation, and Parking:

1. Vehicular access shall be taken from an abutting alley or adjacent street rather than from North Highway 101 where feasible.
2. Where vehicular access off of North Highway 101 is necessary, access shall be limited to one point for each 300 feet of frontage or one point per parcel if frontage is less than 300 feet.
3. All alleys shall be maintained for vehicular access and/or pedestrian paths, and shall not be vacated.
4. Access points to adjoining lots shall be shared wherever feasible. Reciprocal ingress and egress, circulation, and parking arrangements shall be required to facilitate the ease of vehicular movement between adjoining properties where feasible;
5. Sidewalks shall be located along the west side of North Highway 101 and other street frontages per streetscape concepts and circulation improvement standards under this Specific Plan;
6. Where feasible, on-site parking areas shall be located off the alley or to the rear of a building. On-site parking areas abutting all local roads or North Highway 101 shall be screened by earth mounding, landscaping, low decorative walls, or a combination of any of these methods, three (3) feet in height.

d. Auxiliary Structures/Equipment and Utilities:

1. All roof appurtenances including, but not limited to, air conditioning units, and mechanical/electrical equipment shall be shielded and architecturally screened from view from on-site parking areas, adjacent public streets and adjacent properties.
2. All ground-mounted mechanical/electrical equipment, including heating and air conditioning units, and trash receptacle areas, shall be completely screened from surrounding properties by use of a wall, fence, or landscaping, or shall be enclosed within a building.
3. All utility connections shall be designed to coordinate with the architectural elements of the site so as not to be exposed except where necessary. Pad-mounted transformers and/or meter box locations shall be included in the site plan with an appropriate screening treatment. All new and existing utility connections within the boundaries of the project shall be placed underground, with the exception of existing overhead power transmission lines with voltage levels exempted by the Municipal Code and long distance and main trunk communications facilities. Transformer, terminal boxes, meter cabinets, pedestals, ducts and other facilities may be placed above ground provided they are screened.
4. Trash receptacles shall be located behind the building, where possible. In all locations, trash receptacles shall be enclosed by a six-foot high decorative masonry wall with view-obstructing gates.

5. Outdoor storage areas, when approved by minor use permit, shall be located to the rear of a building and shall be enclosed by solid decorative masonry walls and view-obstructing gates, both to be not less than six (6) feet in height to adequately screen such areas from view. Reasonable substitutions such as masonry, wood or metal pilasters with wrought iron or vinyl-clad chain link and view obscuring material may be approved during Design Review.

e. Landscaping:

1. Site landscaping shall include a combination of trees, shrubbery, vines, or groundcovers all of which shall be drought-tolerant, where feasible. Landscaping in parking areas shall be predominantly trees to provide shade and visual relief.
2. Where feasible, landscaping in parking areas shall include one fifteen (15) gallon tree for every four (4) parking spaces distributed throughout the parking area.
3. All landscaped areas shall be watered by an automatic irrigation system with underground piping (excluding drip or soaker irrigation systems) and regularly maintained in a healthy and thriving condition free of weeds, trash, debris.
4. All legal lots 10,000 square feet or less in size shall be required to provide a minimum of 10 percent landscaping. All legal lots exceeding 10,000 square feet in size shall be required to provide a minimum of 15 percent landscaping. Pedestrian plazas or decorative landscaping, which include site amenities such as sculpture, fountains, planters, enhanced paving, etc., may be counted towards meeting the respective as landscaping minimum, up to a maximum of 50 percent of the respective landscaping minimum.
5. Landscaping shall be in accordance with Section 4.8, Landscape Recommendations.

f. Lighting:

1. Lighting placed upon the building shall be architecturally integrated.
2. All light sources shall be shielded in such a manner that the light is directed away from streets or adjoining properties. Illuminators should be integrated within the architecture of the building. Freestanding lamp posts shall be no taller than eighteen (18) feet. The intensity of light at the boundary of the N-CRM-1 Zone shall not exceed seventy-five (75) foot lamberts from the source of light.

g. Architectural Features, Accessory Structures and Walls/Fences:

1. Accessory structures shall not be located in front of or on the street side of the main building(s).
2. Accessory structures shall meet the setback requirements for main building(s).

3. Between any street property line and a principal structure on that property, a wall or fence shall not exceed four (4) feet in height. Where a retaining wall is used to increase usable lot area the height of said retaining wall, if located between any street property line and a principal structure, shall not exceed four (4) feet.
4. The use of chain link, barbed wire and razor wire fencing within public view shall be prohibited.

h. Condominium Conversion

The conversion of a project to condominium ownership shall meet all requirements of the zone to the maximum extent possible within the constraints of the existing development. In no case shall the requirements for the screening of storage/sales areas or mechanical equipment be waived.

i. Design Recommendations

Applicable provisions of the Specific Plan Design Recommendations, Chapter 4 of this Specific Plan, shall apply to proposed development under this zone. If a conflict arises between provisions of the design recommendations and the development standards of this zone, the development standards shall take precedence.

j. Signage Regulations

Except as otherwise specified in this Section and Chapter 4 of this Specific Plan, the signage standards, and review and approval procedures of Chapters 23.08 and 30.60 of the Encinitas Municipal Code shall apply for all signs within the North 101 Corridor Specific Plan.

k. Freestanding A-frame signage

In addition to other types of permitted signage, freestanding A-frame signs shall be allowed, subject to the following standards:

1. one (1) per business.
2. maximum five (5) feet in height by three (3) feet in width.
3. maximum of two (2) faces per sign, back-to-back.
4. no external or internal illumination.
5. not permanently affixed to any object, structure or the ground, including utility poles, light poles, trees or any merchandise or products displayed outside of buildings.
6. portable, self-supporting, stable and weighted or constructed to withstand overturning by wind or contact.
7. not on display during non-business hours.

8. located directly in front of the building/business it is advertising.
9. not placed in such a way as to interfere with pedestrian or vehicular sight lines or corner clear zone requirements as specified by the City.
10. not placed in such a way as to obstruct access to a public sidewalk, public street, driveway, parking space, fire door, fire escape, handicapped access or in such a way that it obstructs free passage over any public right-of-way.
11. not obscure or interfere with the effectiveness of any official notice or public safety device.
12. maintain at least a four (4) foot width for pedestrian clearance over the entire length of the sidewalk or the public right-of-way in front of the business, free of all obstructions such as planter areas, fire hydrants, street lights, other street furniture, on-street parking overhang, and any other approved public right-of-way dining area.

I. Placement of A-frame Signs in the Public Right-of-Way

In addition to meeting the above standards, A-frame signs which encroach into the public right-of-way are allowed with the issuance of an encroachment permit from the Engineering Services Department. Encroachment permits for A-frame signs in the public right-of-way shall require the submittal of the following information and are subject to the following limitations:

1. Submit a brief description of the proposed encroachment and its proposed location in the public right-of-way.
2. Submit a site plan, in a form acceptable to the Engineering Services Department and the Community Development Department, showing local conditions, including street and sidewalk width, and location/dimensions of all street furniture and elements on the sidewalk.
3. The description and site plan shall be reviewed and accepted by the Community Development Department prior to the issuance of an encroachment permit by the Engineering Services Department.
4. A liability insurance policy naming the City as additionally insured in the amount of \$1,000,000 shall be on file with the Engineering Services Department and the Community Development Department.
5. An issued encroachment permit may be revoked by the City after a thirty (30) day notice.

E. Zone: Commercial Residential Mixed 2 (N-CRM-2)

This zone is intended to provide an extension of the neighborhood-serving commercial node on Leucadia Boulevard at Vulcan Avenue. The zone provides for a variety of development opportunities including 1) stand alone commercial (excluding auto-relates uses), 2) mixed use; with commercial use types to be generally neighborhood-serving as per the permitted, minor use permit, and major use permit use list of this section, or 3) stand alone residential. This zone applies to properties fronting on Vulcan Avenue between the commercial properties fronting on Leucadia Boulevard at Vulcan Avenue and the Riviera Mobile Home Park.

Commercial allows commercial uses which generally serve the needs of local, neighborhood residents, and which are compatible with a residential neighborhood.

Mixed Use allows commercial and residential uses at a maximum density of 15.0 dwelling units per net acre, on the same property or in the same structure with the intent of providing opportunities for housing and live/work or artisan loft arrangements, while also mitigating the impacts between commercial and residential uses.

It is the intention of the N-CRM-2 Zone to allow for functional and physical integration of project components of mixed uses. Consideration will be given to joint use of parking, sizes of residential units and overall architectural design when considering approvals for mixed use developments.

Residential allows single-family detached or multi-family detached and attached residential units, at a maximum density of 15.0 dwelling units per net acre.

The following principal uses shall be permitted in the N-CRM-2 Zone and are identified as either permitted, or requiring a minor use permit or major use permit. All other principal uses not specifically listed are prohibited.

1. Permitted Uses:

- Accessory Building
- Antique Sales, Retail
- Appliance Sales & Repairs (Household and Small Appliances)
- Aquarium, Commercial
- Art Gallery
- Artisan/Craftsman(Sales & Studio)
- Bakery (Retail)
- Bank/Savings & Loan
- Barber & Beauty Shop (Cosmetologist)
- Bicycle Sales, Rental and Service
- Book Sales
- Candle Shop
- Candy and Confectionery Sales
- Catering Service
- Charitable Bins and Depositories (accessory)
- Clothing Rental
- Coins, Purchase and Sales
- Computer Sales and Service
- Conservatory of Music

Cosmetic Design Studio
Costume Rentals
Dairy Store
Dance Studio
Delicatessen
Dressmaking Shop
Drug Store
Dry Cleaning, Retail and Self-Service
Dwelling Unit, Caretaker - accessory to the principal use
Dwelling Unit, One-Family¹
Dwelling Unit, Two-Family¹
Dwelling Unit, Three-Family¹
Dwelling Unit, Multiple Family¹
Educational Institution, Public
Employment Agency
Family Day Care Home, Small (Six or fewer persons, accessory to dwelling only)
Family Day Care Home, Large (Seven to twelve persons, accessory to dwelling only)
Finance Company
Fire Station
Floor Covering, Retail
Florist Shop
Furniture Sales
Gem/Precious Stone, Retail
Gift Shop
Glass and Mirrors, Retail
Glass Edging and Beveling
Glass Studio (Stained and Others)
Group Home
Hair Salon
Hardware Store
Hat Shop
Health Food Store
Hobby Supply Shop
Home Occupations
Ice Cream Parlor
Interior Decorating Service
Jewelry Sales
Lapidary Shop, Retail (Gems)
Laundromat
Laundry
Lawnmower Sales and Service
Library
Locksmith
Mail Order House
Medical/Dental Clinic
Medical/Dental Office
Medical Equipment Sales
Music/Record Store
Newsstand
Notions and Dry Goods Store
Office (Business and Professional)
Office Equipment/Supplies Sales
Optical Products Sales
Orthopedic Devices Sales

¹ All detached or attached dwelling units in residential-only or mixed-use developments must be constructed on a legally subdivided lot or must be subdivided to permit ownership of airspace in the form of a dwelling unit with an undivided share in common elements.

3.0 Use and Development Regulations

Pet Shop/Pet Grooming (grooming only, no kennels or boarding)
Pharmacy
Photocopy Shop
Photofinishing, Retail
Photographic Studio
Photographic Supplies
Photograving and Finishing
Plumbing Shop
Postal Annex, Private Ownership
Pottery Sales
Printing
Public Utilities: Office
Radio/Television Sales
Real Estate Office
Reducing Salon
Restaurant (no alcohol sales; outdoor dining permitted by design review)¹
Retail Sales²
Schools, Public: (Elementary, Jr. High, High School)
Scientific Instruments Sales
Second Hand Dealer
Second Hand Store
Shoe Repair/Sales
Sidewalk Dining (requires an encroachment permit)¹
Silk Screen Printing Shop
Sporting Goods Sales
Stationery Store
Stone (Precious) Manufacturing
Surf Shop Retail and Rental (no manufacturing)
Tailor Shop
Telephone Answering Service
Tile Sales
Utility Lines - Sewer, Gas, Petroleum, Telephone, Electric
Veterinarian (No Kennel)
Video Sales & Rental
Wearing Apparel Shop

2. Minor Use Permits

Auction House
Bed and Breakfast
Club, Athletic or Recreational Courts, Commercial (Badminton, Tennis, Racquetball, Others)
Day Care Center
Gymnasium, Public Health Club
Horticultural Services
Janitorial Services
Nurseries, Horticultural
Outdoor Sales Parks and Recreational Areas
Recording Studio
Recreational Facilities, Public and Private
Restaurant (with alcohol sales)¹
Video Production Studio
Upholstery Installation

¹ See Section E.4.d Sidewalk Dining in this chapter for additional provisions.

² Neighborhood-serving retail sales uses consistent with the intent of the N-CRM-2 Zone are permitted.

3. Major Use Permits

Athletic Field, Public
Cellular Facility
Church/Temple/Religious Institution
Educational Institution, Private
Electrical Distribution Substation
Gas Distribution, Meter and Control Station
Schools, Private: (Elementary, Jr. High, High School)
Schools, Technical
Transitional Housing

4. Development Standards - Commercial or Mixed-Use Developments:

a. General Standards:

Net Lot Area:	3,950 net square feet minimum
Lot Width:	40 feet
Lot Depth:	90 feet
Front Yard Setback:	0 feet
Side Yard Setback for each interior side:	0 feet
Side Yard Setback street side:	0 feet
Rear Yard Setback:	10 feet ¹
Lot Coverage (maximum):	50 percent
Building Height maximum:	33 feet or three stories, whichever is less.
Landscaping:	10/15 percent ² (See Landscape Recommendations, Section 4.8)
Off-Street Parking:	See Parking Requirements, Section 3.3
Floor Area Ratio:	No requirement

b. Mixed Use Site Planning:

1. Separate entrances shall be required for residential and commercial uses when occupying the same structure. (This does not preclude secondary interior access between commercial and residential uses, in addition to the required separate accesses, provided that residential access to or through commercial uses avoids unsafe commercial areas.)
2. Residential uses shall be located either above, behind or beside the primary commercial use and shall not occupy ground floor building frontage area along Vulcan Avenue.
3. All detached or attached dwelling units in mixed-use developments must be constructed on a legally subdivided lot or must be subdivided to permit ownership of airspace in the form of a dwelling unit with an undivided share in common elements.
4. Residential uses shall not exceed 50 percent of the gross building floor area for the development site, unless authorized through the approval of a Major Use Permit.
5. Residential densities shall be calculated pursuant to Section 30.16.010.B 1. and 2. of Title 30 of the Encinitas Municipal Code.

¹ A minimum ten (10) foot rear or side yard setback shall be required where any portion of the rear or side yard immediately abuts a residentially zoned property.

² All legal lots 10,000 square feet or less in size shall be required to provide a minimum of 10 percent landscaping. All legal lots exceeding 10,000 square feet in size shall be required to provide a minimum of 15 percent landscaping. Pedestrian plazas or decorative landscaping, which include site amenities such as sculpture, fountains, planters, enhanced paving, etc., may be counted towards meeting the respective landscaping minimum, up to a maximum of 50 percent of the respective landscaping minimum.

c. Additional Setbacks and Separation from Adjacent Land Uses:

masonry wall of no less than six (6) feet in height is required along the property line between the commercial/mixed use development and the residentially-zoned property, and may be increased to eight (8) feet to resolve any noise or visual impacts. Said wall shall be measured from the highest finished grade at the property line, and shall be erected and maintained as separation between the commercial and residential uses. Said wall shall be stepped down to no more than four (4) feet in height within 10 feet of the front property line.

d. Sidewalk Dining:

1. Outdoor/sidewalk dining areas that encroach into the public right-of-way shall require an encroachment permit from the Engineering Services Department and must maintain a minimum 4-foot clearance which excludes planter areas, fire hydrants, street lights, other street furniture, and on-street auto parking overhang, and any approved A-frame signage.
2. Encroachment permits for outdoor/sidewalk dining within public rights-of-way require the following information and are subject to the following limitations:
 - a. Submit a brief description of the proposed encroachment and its proposed location in the public right-of-way.
 - b. Submit a site plan, in a form acceptable to the Engineering Services Department and the Community Development Department, showing local conditions, including street and sidewalk width, and location/dimensions of all street furniture and elements on the sidewalk.
 - c. Specify the number of tables and seating requested and not-to-exceed-amount.
 - d. An issued permit may be revoked by the City after a 30-day notice.
 - e. Hours of operation shall be pre-determined through the encroachment permit and limited to an associated adjacent eating and drinking establishment.
 - f. No sound amplification device, musical instrument or sound reproduction device shall be operated or used with outdoor dining, and outdoor lighting shall comply with performance standards, and be approved by the City.
 - g. Prior to issuance and approval of a permit, a finding shall be made by the Community Development Director or designee that the outdoor dining will not adversely affect the neighborhood nor be detrimental to persons residing, visiting or working in the area.
 - h. A liability insurance policy naming the city as additionally insured for \$1,000,000 shall be on file with the Engineering Services Department and the Community Development Department.

5. Development Standards - Residential-Only Developments:

- a. In residential-only developments, except as otherwise specified herein, standards pursuant to Chapter 30.16, R-15 Zone of Title 30 of the Encinitas Municipal Code shall apply.
- b. Maximum building height is thirty-three (33) feet or three (3) stories, whichever is less.
- c. All detached or attached dwelling units in residential-only developments must be constructed on a legally subdivided lot or must be subdivided to permit ownership of airspace in the form of a dwelling unit with an undivided share in common elements.

6. General Development Standards - Commercial, Mixed-Use, or Residential-Only Developments:

a. General Site Planning

1. Site planning shall include consideration of adjoining parcels in terms of building configuration, building design, and scale of landscaping materials, circulation/parking configuration and access.
2. All commercial uses, including storage, shall be located within an enclosed building unless authorized by a Minor Use Permit or as otherwise permitted by this Specific Plan.

b. Building Design and Use:

1. Visual relief shall be provided for linear buildings by the use of architectural projections or recessed areas which create the appearance of smaller adjoining buildings.
2. Energy consumption in buildings should be minimized using the following features where feasible:
 - Cogeneration
 - Solar access
 - South facing windows with eave coverage
 - Double glazed windows
 - Deciduous shade trees
 - Good ventilation
 - Efficient lighting
 - Day lighting
3. All signage associated with the buildings and site shall be integrated in terms of both architecture and color and shall comply with Section 4.5.6 of the specific plan.

c. Access, Circulation, and Parking:

1. Vehicular access off of Vulcan Avenue shall be limited to one access point for each 300 feet of frontage, or one point per parcel if frontage is less than 300 feet.

2. Where feasible, on-site parking areas shall be located to the rear of buildings. On-site parking abutting Vulcan Avenue shall be screened by earth mounding, landscaping, low decorative walls, or a combination of any of these methods, three (3) feet in height. Access points between adjoining lots are strongly encouraged to be shared where feasible.

d. Auxiliary Structures/Equipment and Utilities:

1. All roof appurtenances including, but not limited to, air conditioning units, and mechanical/electrical equipment shall be shielded and architecturally screened from view from on-site parking areas, adjacent public streets and adjacent properties.
2. All ground-mounted mechanical/electrical equipment, including heating and air conditioning units, and trash receptacle areas, shall be completely screened from surrounding properties by use of a wall, fence, or landscaping, or shall be enclosed within a building.
3. All utility connections shall be designed to coordinate with the architectural elements of the site so as not to be exposed except where necessary. Pad-mounted transformers and/or meter box locations shall be included in the site plan with an appropriate screening treatment. All new and existing utility connections within the boundaries of the project shall be placed underground, with the exception of existing overhead power transmission lines with voltage levels exempted by the Municipal Code and long distance and main trunk communications facilities. Transformer, terminal boxes, meter cabinets, pedestals, ducts and other facilities may be placed above ground provided they are screened.
4. Trash receptacles shall be located behind the building, where possible. In all locations, trash receptacles shall be enclosed by a six-foot high decorative masonry wall with view-obstructing gates.
5. Outdoor storage areas, when approved by minor use permit, shall be located to the rear of a building and shall be enclosed by solid decorative masonry walls and view-obstructing gates, both to be not less than six (6) feet in height to adequately screen such areas from view. Reasonable substitutions such as masonry, wood or metal pilasters with wrought iron or vinyl-clad chain link and view obscuring material may be approved during Design Review.

e. Landscaping:

1. Site landscaping shall include a combination of trees, shrubbery, vines, or groundcovers all of which shall be drought-tolerant, where feasible. Landscaping in parking areas shall be predominantly trees to provide shade and visual relief.
2. Where feasible, landscaping in parking areas shall include one fifteen (15) gallon tree for every four (4) parking spaces distributed throughout the parking area.

3. All landscaped areas shall be watered by an automatic irrigation system with underground piping (excluding drip or soaker irrigation systems) and regularly maintained in a healthy and thriving condition free of weeds, trash, debris.
4. All legal lots 10,000 square feet or less in size shall be required to provide a minimum of 10 percent landscaping. All legal lots exceeding 10,000 square feet in size shall be required to provide a minimum of 15 percent landscaping. Pedestrian plazas or decorative landscaping, which include site amenities such as sculpture, fountains, planters, enhanced paving, etc., may be counted towards meeting the respective as landscaping minimum, up to a maximum of 50 percent of the respective landscaping minimum.
5. Landscaping shall be in accordance with Section 4.8, Landscape Recommendations.

f. Lighting:

1. Lighting placed upon the building shall be architecturally integrated.
2. All light sources shall be shielded in such a manner that the light is directed away from streets or adjoining properties. Illuminators should be integrated within the architecture of the building. Freestanding lamp posts shall be no taller than eighteen (18) feet. The intensity of light at the boundary of the N-CRM-2 Zone shall not exceed seventy-five (75) foot lamberts from the source of light.

g. Architectural Features, Accessory Structures and Walls/Fences:

1. Accessory structures shall not be located in front of the main building(s).
2. Accessory structures shall meet the setback requirements for main building(s) for the front yard area.
3. Architectural features, such as canopies, eaves, steps, balconies, stairways, and others which do not create inhabitable space may project into minimum front and rear setbacks no more than three (3) feet; and may project into minimum side-yard set backs no more than two (2) feet with a maximum six (6) foot vertical dimension.
4. Between any street property line and a principal structure on that property, a wall or fence shall not exceed four (4) feet in height. Where a retaining wall is used to increase usable lot area, and the height of said retaining wall, if located between any street property line and a principal structure, shall not exceed four (4) feet.
5. The use of chain link, barbed wire and razor wire fencing within public view shall be prohibited.

h. Condominium Conversion

The conversion of a project to condominium ownership shall meet all requirements of the zone to the maximum extent possible within the constraints of the existing development. In no case shall the requirements for the screening of storage/sales areas or mechanical equipment be waived.

i. Design Recommendations

Applicable provisions of the Specific Plan Design Recommendations, Chapter 4 of this Specific Plan, shall apply to proposed development under this zone. If a conflict arises between provisions of the design recommendations and the development standards of this zone, the development standards shall take precedence.

j. Signage Regulations

Except as otherwise specified in this Section and Chapter 4 of this Specific Plan, the signage standards, and review and approval procedures of Chapters 23.08 and 30.60 of the Encinitas Municipal Code shall apply for all signs within the North 101 Corridor Specific Plan.

k. Freestanding A-frame signage

In addition to other types of permitted signage, freestanding A-frame signs shall be allowed, subject to the following standards:

1. one (1) per business.
2. maximum five (5) feet in height by three (3) feet in width.
3. maximum of two (2) faces per sign, back-to-back.
4. no external or internal illumination.
5. not permanently affixed to any object, structure or the ground, including utility poles, light poles, trees or any merchandise or products displayed outside of buildings.
6. portable, self-supporting, stable and weighted or constructed to withstand overturning by wind or contact.
7. not on display during non-business hours.
8. located directly in front of the building/business it is advertising.
9. not placed in such a way as to interfere with pedestrian or vehicular sight lines or corner clear zone requirements as specified by the City.

10. not placed in such a way as to obstruct access to a public sidewalk, public street, driveway, parking space, fire door, fire escape, handicapped access or in such a way that it obstructs free passage over any public right-of-way.
11. not obscure or interfere with the effectiveness of any official notice or public safety device.
12. maintain at least a four (4) foot width for pedestrian clearance over the entire length of the sidewalk or the public right-of-way in front of the business, free of all obstructions such as planter areas, fire hydrants, street lights, other street furniture, on-street parking overhang, and any other approved public right-of-way dining area.

I. Placement of A-frame Signs in the Public Right-of-Way

In addition to meeting the above standards, A-frame signs which encroach into the public right-of-way are allowed with the issuance of an encroachment permit from the Engineering Services Department. Encroachment permits for A-frame signs in the public right-of-way shall require the submittal of the following information and are subject to the following limitations:

1. Submit a brief description of the proposed encroachment and its proposed location in the public right-of-way.
2. Submit a site plan, in a form acceptable to the Engineering Services Department and the Community Development Department, showing local conditions, including street and sidewalk width, and location/dimensions of all street furniture and elements on the sidewalk.
3. The description and site plan shall be reviewed and accepted by the Community Development Department prior to the issuance of an encroachment permit by the Engineering Services Department.
4. A liability insurance policy naming the City as additionally insured in the amount of \$1,000,000 shall be on file with the Engineering Services Department and the Community Development Department.
5. An issued encroachment permit may be revoked by the City after a thirty (30) day notice.

F. Zone: Visitor-Serving Commercial (N-VSC)

This zone is intended to provide for commercial activities that serve the needs of persons visiting the city for recreation and business.

1. Except as otherwise specified below, all provisions of Title 30 of the Encinitas Municipal Code applicable to the VSC zone shall apply under the N-VSC zone.
2. The Landscape Recommendations of Section 4.8 of this Specific Plan shall apply to all new and rehabilitated landscape projects with a landscaped area of greater than 1,000 square feet.
3. Applicable provisions of the Specific Plan Design Recommendations, Chapter 4 of this Specific Plan, shall apply to proposed development under this zone. If a conflict arises between provisions of the design recommendations and the development standards of this zone, the development standards shall take precedence.
4. The Floor Area Ratio (FAR) requirements of Section 30.20.010.A of Title 30 of the Encinitas Municipal Code shall not apply to the N-VSC zone.
5. Signage Regulations: Except as otherwise specified in this Section and Chapter 4 of this Specific Plan, the signage standards, and review and approval procedures of Chapters 23.08 and 30.60 of the Encinitas Municipal Code shall apply for all signs within the North 101 Corridor Specific Plan.

a. Freestanding A-frame signage

In addition to other types of permitted signage, freestanding A-frame signs shall be allowed, subject to the following standards:

1. one (1) per business.
2. maximum five (5) feet in height by three (3) feet in width.
3. maximum of two (2) faces per sign, back-to-back.
4. no external or internal illumination.
5. not permanently affixed to any object, structure or the ground, including utility poles, light poles, trees or any merchandise or products displayed outside of buildings.
6. portable, self-supporting, stable and weighted or constructed to withstand overturning by wind or contact.

7. not on display during non-business hours.
8. located directly in front of the building/business it is advertising.
9. not placed in such a way as to interfere with pedestrian or vehicular sight lines or corner clear zone requirements as specified by the City.
10. not placed in such a way as to obstruct access to a public sidewalk, public street, driveway, parking space, fire door, fire escape, handicapped access or in such a way that it obstructs free passage over any public right-of-way.
11. not obscure or interfere with the effectiveness of any official notice or public safety device.
12. maintain at least a four (4) foot width for pedestrian clearance over the entire length of the sidewalk or the public right-of-way in front of the business, free of all obstructions such as planter areas, fire hydrants, street lights, other street furniture, on-street parking overhang, and any other approved public right-of-way dining area.

b. Placement of A-frame Signs in the Public Right-of-Way

In addition to meeting the above standards, A-frame signs which encroach into the public right-of-way are allowed with the issuance of an encroachment permit from the Engineering Services Department. Encroachment permits for A-frame signs in the public right-of-way shall require the submittal of the following information and are subject to the following limitations:

1. Submit a brief description of the proposed encroachment and its proposed location in the public right-of-way.
2. Submit a site plan, in a form acceptable to the Engineering Services Department and Community Development Department, showing local conditions, including street and sidewalk width, and location/dimensions of all street furniture and elements on the sidewalk.
3. The description and site plan shall be reviewed and accepted by the Community Development Department

prior to the issuance of an encroachment permit by the Engineering Services Department.

4. A liability insurance policy naming the City as additionally insured in the amount of \$1,000,000 shall be on file with the Engineering Services Department and the Community Development Department.
5. An issued encroachment permit may be revoked by the City after a thirty (30) day notice.

G. Zone: Limited Visitor-Serving Commercial (N-L-VSC)

This zone is intended to provide for hotel/motel uses as the primary use, with uses specifically intended to serve the needs of persons visiting the City as ancillary uses.

1. Except as otherwise specified below, all provisions of Title 30 of the Encinitas Municipal Code applicable to the L-VSC zone shall apply under the N-L-VSC zone.
2. The Landscape Recommendations of Section 4.8 of this Specific Plan shall apply to all new and rehabilitated landscape projects with a landscaped area of greater than 1,000 square feet.
3. Applicable provisions of the Specific Plan Design Recommendations, Chapter 4 of this Specific Plan, shall apply to proposed development under this zone. If a conflict arises between provisions of the design recommendations and the development standards of this zone, the development standards shall take precedence.
4. The Floor Area Ratio (FAR) requirements of Section 30.20.10.A of Title 30 of the Encinitas Municipal Code shall not apply to the N-L-VSC zone.
5. Signage Regulations: Except as otherwise specified in this Section and Chapter 4 of this Specific Plan, the signage standards, and review and approval procedures of Chapters 23.08 and 30.60 of the Encinitas Municipal Code shall apply for all signs within the North 101 Corridor Specific Plan.

a. Freestanding A-frame signage

In addition to other types of permitted signage, freestanding A-frame signs shall be allowed, subject to the following standards:

1. one (1) per business.
2. maximum five (5) feet in height by three (3) feet in width.
3. maximum of two (2) faces per sign, back-to-back.
4. no external or internal illumination.
5. not permanently affixed to any object, structure or the ground, including utility poles, light poles, trees or any merchandise or products displayed outside of buildings.
6. portable, self-supporting, stable and weighted or constructed to withstand overturning by wind or contact.

7. not on display during non-business hours.
8. located directly in front of the building/business it is advertising.
9. not placed in such a way as to interfere with pedestrian or vehicular sight lines or corner clear zone requirements as specified by the City.
10. not placed in such a way as to obstruct access to a public sidewalk, public street, driveway, parking space, fire door, fire escape, handicapped access or in such a way that it obstructs free passage over any public right-of-way.
11. not obscure or interfere with the effectiveness of any official notice or public safety device.
12. maintain at least a four (4) foot width for pedestrian clearance over the entire length of the sidewalk or the public right-of-way in front of the business, free of all obstructions such as planter areas, fire hydrants, street lights, other street furniture, on-street parking overhang, and any other approved public right-of-way dining area.

b. Placement of A-frame Signs in the Public Right-of-Way

In addition to meeting the above standards, A-frame signs which encroach into the public right-of-way are allowed with the issuance of an encroachment permit from the Engineering Services Department. Encroachment permits for A-frame signs in the public right-of-way shall require the submittal of the following information and are subject to the following limitations:

1. Submit a brief description of the proposed encroachment and its proposed location in the public right-of-way.
2. Submit a site plan, in a form acceptable to the Engineering Services Department and Community Development Department, showing local conditions, including street and sidewalk width, and location/dimensions of all street furniture and elements on the sidewalk.
3. The description and site plan shall be reviewed and accepted by the Community Development Department

3.0 Use and Development Regulations

prior to the issuance of an encroachment permit by the Engineering Services Department.

4. A liability insurance policy naming the City as additionally insured in the amount of \$1,000,000 shall be on file with the Engineering Services Department and the Community Development Department.
5. An issued encroachment permit may be revoked by the City after a thirty (30) day notice.

3.1.3 ZONE: PUBLIC/SEMI-PUBLIC (N-P/SP)

This zone is intended to provide for activities operated by the City, County, or other governmental agencies such as school districts, water districts, and sewer districts; other semi-public uses, such as hospitals and other private institutions may also be included.

All provisions of Title 30 of the Encinitas Municipal Code applicable to the P/SP zone shall apply under the N-P/SP zone.

3.1.4 ZONE: HISTORIC PARK (N-HP)

This zone is intended to provide for the Leucadia Roadside Park which is historically significant. The park has been set aside for preservation as a public open space and passive recreational area. No private development may occur within this zone. Park development shall be approved by minor use permit, processed as required by Title 30 of the Encinitas Municipal Code. Refer to Section 4.5.8 for applicable design recommendations.

Except as otherwise specified herein, all provisions of Title 30 of the Encinitas Municipal Code applicable to the ER/OS/P zone shall apply under the N-HP zone.

- a. **Off-Street Parking:** None required.
- b. **Landscaping:** All existing eucalyptus and cypress trees shall be preserved and maintained to the extent possible. Dead or diseased trees shall be replaced with drought tolerant species with similar characteristics.
- c. **Lighting:** Restoration or replacement of existing historic street lights adjacent to street frontages is strongly encouraged.
- d. **Building Design and Use:** Private development is prohibited.
- e. **Design Recommendations:** Applicable provisions of the Specific Plan Design Recommendations, Chapter 4 of this Specific Plan, shall apply to proposed development under this zone. If a conflict arises between provisions of the design recommendations and the development standards of this zone, the development standards shall take precedence.

3.1.5 ZONE: TRANSPORTATION CORRIDOR (N-TC)

This zone is intended to preserve public transportation rights-of-way to ensure adequate land is available for future transportation modes and compatible accessory uses, including public recreational trails, public parking, public parks, etc. In the North Highway 101 Corridor Specific Plan, the N-TC zone applies to the railroad right-of-way owned by the North San Diego County Transit District.

The following uses shall be permitted in the N-TC Zone. All other uses are prohibited:

1. **Permitted Uses:** The development of all permitted uses shall be subject to Design Review.
 - Bus Stop (drop-off & pick-up only)¹
 - Linear Park Facilities¹
 - Utility Lines - Sewer, Gas, Petroleum, Telephone, Electric²
 - Public Parking Bays¹
 - Public Pedestrian/Bicycle/RecreationaPaths¹
 - Rail Transportation Service

3.1.6 SPECIAL PURPOSE OVERLAY ZONES

If applicable, the Special Purpose Overlay Zones of Chapter 30.34 of Title 30 of the Encinitas Municipal Code, including the Coastal Bluff Overlay Zone, the Hillside/Inland Bluff Overlay Zone, the Floodplain Overlay Zone, the Cultural/Natural Resources Overlay Zone, the Agricultural Overlay Zone, the Public Facilities Overlay Zone, and the Scenic/Visual Corridor Overlay Zone shall apply to all properties within the North 101 Corridor Specific Plan area.

¹ Shall be consistent with the Specific Plan Streetscape Design Recommendations and Concept Plans (see Section 4.7) and the Specific Plan Circulation Plan (see Chapter 5).

² New lines shall be placed underground except high voltage electrical transmission lines, as per City of Encinitas Municipal Code Section 30.20.010F

3.2.3 Parking Standards

3.2 PARKING

3.2.1 INTRODUCTION

Parking strategies for the North 101 Corridor Specific Plan area are important for maintaining customer convenience in commercial use areas and avoiding overflow into residential neighborhoods. While an adequate number of spaces is the desired result, parking strategies must work within the parameters of the Specific Plan to be effective.

Requiring off-street parking is the usual method for providing adequate parking for land uses. With this strategy, development is required to provide parking on each development site per specified parking ratios. In most cases this is effective, and for most of the North 101 Corridor Specific Plan area, off-street parking requirements are adequate to satisfy the need. However, some areas of the North 101 Corridor do not have adequate off-street parking. Small lot sizes, existing development patterns and previous development standards did not adequately provide for parking. Applying only the off-street parking strategy in the commercial corridor does not recognize the inability of many of the sites to meet this standard and could stifle economic development.

In order to provide adequate parking and maximize the benefits of the specific plan, special parking strategies have been developed for certain zones. Any Specific Plan zone not specifically addressed shall meet the citywide parking standards of Chapter 30.54 of Title 30 of the Encinitas Municipal Code.

3.2.2 PARKING STRATEGIES

A. Residential Zones (N-R3, N-R8, N-R11, N-R15, N-R20, and N-R25)

It is expected that development sites in the residential zones can accommodate off-street parking. The application of the off-street parking requirements of Chapter 30.54 of Title 30 of the Encinitas Municipal Code is adequate for each residential zoning designation, and there is no need for additional special parking strategies in these zones. Modified parking ratios as described under Section 3.2.3.C. below shall apply.

B. Commercial and Mixed-Use Zones (N-VSC, N-L-VSC, N-CM-1, N-CM-2, N-CM-3, N-CRM-1 and N-CRM-2 Zones)

The Commercial and Mixed-Use Zones will incorporate several strategies to provide parking. Existing circumstances for properties in these zones dictate that parking must be addressed in ways other than just off-street parking requirements. Specialized parking strategies are provided to meet parking needs while maintaining district design and character. The intent is to apply a combination of these strategies, as applicable, to the total parking requirement for each individual development use.

1. The first strategy is a redefined set of parking ratios for commercial and office uses. The commercial parking ratios in the citywide zoning code are based on the expectations of contemporary development on relatively large lots with large building occupancies available. However, the common pattern in the North Highway 101 Corridor Specific Plan area is small lot sizes and related small building occupancies.

2. The second strategy is to allow on-street parking which is immediately adjacent to a development site to count toward the parking requirement for uses on that site. Usually, parking requirements must be satisfied exclusively by off-street (on-site) parking. However, in the North 101 Corridor Specific Plan area, the pattern of development is largely established and reliable predictions of future build-out traffic levels are provided. On-street parking on North Highway 101, Vulcan Avenue and side streets, once provided according to the provisions of this Plan, will not be removed. This provides a reliable resource of parking which, even if time-restricted, should be available for customer use.
3. The third strategy is an incentive for the remodeling and reuse of existing buildings. Citywide standards currently allow existing buildings a parking "break" in that, when an addition is proposed to an existing building, if the development site will not allow the full parking requirement to be provided, the parking requirement must be met only for the floor area of the new addition rather than the whole building. Upon a change of use, however, if the new use has a higher parking ratio requirement than the previous use, citywide standards require the full parking requirement for the new use to be provided. This specific plan allows existing buildings the same parking break as in the current citywide standards, but expands it to apply to any change of use, irrespective of the parking ratio requirement. This applies only insofar as the development site is restricted so as to preclude the provision of additional parking. To the maximum extent possible, based on development standards under the specific plan, the maximum number of parking spaces must be provided up to the number required by the parking ratios. This parking strategy is intended as an economic development strategy.
4. The fourth parking strategy is an incentive for affordable housing. The mixed-use zones allow a limited amount of residential development in conjunction with commercial use. The parking required for these mixed-use units is a maximum of two spaces per dwelling. This specific plan allows a parking reduction for units which are guaranteed affordable to low or very-low income households, as defined under the City's Housing Element, of one space per unit. Note that all parking for residential units in mixed-use developments must be provided off-street (on-site).
5. The final parking strategy for the North 101 Corridor Specific Plan involves the opportunity to develop public parking bays on the east side of North Highway 101, using available railroad right-of-way. The streetscape plan for North Highway 101 includes plans for a linear park and multi-modal recreational path between the railroad tracks and street paving for the highway. The intent is to design the linear park and multi-modal recreational path in a manner that accommodates parallel parking bays, immediately before or after intersections where safe pedestrian crossings of North Highway 101 already exist or are planned. This includes the intersections of North Highway 101 at La Costa Avenue, Grandview Street, Leucadia Boulevard, and Marcheta Street. These parking bays are seen as a general public parking resource, with no particular need for time restriction at this time. This parking increases general public accessibility to the corridor, but should not be allowed to be counted toward the parking requirements of specific developments. Should the City adopt an In-lieu

Fee Parking Program, such parking may be allowed to be counted toward the parking requirements of specific developments.

C. Public/Semi-Public Zone

It is expected that development sites in this zone can accommodate off-street parking. The application of the off-street parking requirements of Chapter 30.54 of Title 30 of the Encinitas Municipal Code is adequate and there is no need for additional special parking strategies in this zone.

D. Ecological Resource/Open Space/Parks- Historic Park Zone (N-HP)

Due to the small size of the Historic Park, no on-site parking exists, nor is planned. Detailed improvement plans for future street improvements surrounding the park shall be consistent with the Streetscape Concepts in Chapter 4 of this Specific Plan and shall address the need for on-street parking.

E Transportation Corridor Zone (TC)

This zone will generally not have any uses which generate the need for parking. However, proposed parking bays on the east side of North Highway 101 are intended to make use of available, railroad right-of-way.

3.2.3 PARKING STANDARDS

A. Purpose and Intent

The provisions set forth in this section are intended to provide for adequate and convenient vehicle parking. The parking requirements set forth herein shall be considered the minimum necessary for specific land uses allowed, within the zones created under this specific plan.

The intent of these provisions is to ensure:

- adequately designed vehicle parking;
- sufficient capacity;
- adequate circulation;
- minimal traffic congestion;
- public safety, and
- convenience.

It shall be the responsibility of land owners, developers, and/or operators to provide and maintain adequate parking meeting the minimum requirements set forth herein.

B. General Provisions

1. Unless otherwise specified herein, the provisions, requirements and standards of Chapter 30.54 of Title 30 of the Encinitas Municipal Code shall apply to all properties in the North 101 Corridor Specific Plan area. In case of conflict between the provisions set forth herein and the provisions of Chapter 30.54 of Title 30 of the Encinitas Municipal Code, the provisions set forth herein shall apply.
2. All required parking shall be off-street, on the same site as the land use it serves, unless otherwise provided by these regulations.

3. A public alley may be used as part of the required width of back-out and maneuvering aisle space for on-site parking which is immediately adjacent to said alley.
4. Off-street parking for a use may be provided on an immediately adjacent development site, by discretionary design review, provided that access from the use to said parking is unimpeded; said parking is not counted toward the required parking of any other land use; and said parking is reserved for the use by a recorded, irrevocable easement to which the City is a party.
5. Joint-use of parking shared between uses may be approved, as provided by Section 30.54.050 of Title 30 of the Encinitas Municipal Code.

C. Schedule of Required Parking

1. The number of required vehicle parking spaces for any land use or combination of uses shall be as set forth in Table 3-1 of the Specific Plan, except as otherwise provided in this Section. For land uses not specifically covered in Table 3-1, the requirements of Chapter 30.54 of Title 30 of the Encinitas Municipal Code shall apply.
2. Housing Affordable to Low and Very-Low Income Households: Residential units in mixed-use projects which are guaranteed to be affordable to low and very-low income households as defined in the City of Encinitas Housing Element, shall provide one off-street parking space per dwelling unit.
3. Commercial and Mixed-Use Zones: The following parking provisions shall only apply to non-residential uses within commercial or mixed-use zones. These provisions apply only insofar as the available area on a development site is restricted, before any new construction, so as to preclude the provision of the required number of parking spaces. In all cases, to the maximum extent possible based on current development standards, the maximum number of parking spaces shall be provided, up to the number required by the schedule of required parking.
 - a. The number of on-street parking spaces which are immediately adjacent to a development site, or the number which will be adjacent after completion of all planned street improvements, whichever is fewer, may be counted toward the required number of parking spaces for non-residential uses on that site. On-street parking on Melrose Avenue and La Veta Avenue shall not be counted towards meeting minimum commercial parking requirements.
 - b. For additions to or increases of gross floor area of existing buildings, or new development proposed to replace existing buildings on the site, the number of required parking spaces is the fewer of either: the total number required for the development site for the combination of uses per the ratios specified in Table 3-1; or the number of existing parking spaces on the development site before new development, plus additional

spaces as required by the ratios specified in Table 3-1 for the proposed addition or increase in gross floor area.

- c. For any change of use in an existing building, not related to an addition to or increase of gross building floor area, the number of required parking spaces is the fewer of either; the total number required for the development site for the combination of uses per the ratios specified in Table 3-1; or the maximum number that the development site together with its share of adjacent on-street parking can yield under current parking and site development standards.

Table 3-1

Required Parking Ratios

Mixed Use Residential Dwellings:	
<u>Use Type</u>	<u>Spaces Required</u>
Studio units- 1 bedroom units	1.5 spaces per unit ¹
2 or more bedroom units	2.0 spaces per unit ¹
Office and Commercial Developments	
<u>Use Type</u>	<u>Spaces Required</u>
General Offices; professional services	1 space for each 300 sq. ft. of gross building area
Personal service commercial uses, including beauty shops, hair salons, nail salons, dry cleaners, etc.	1 space for each 300 sq. ft. of gross building area
Other individual retail uses and commercial services except as otherwise specified.	1 space for each 300 sq. ft. of gross building area
Restaurants, bars, lounges	1 space for each 75 sq. ft. of net building floor area ² and outdoor dining area
Take-out restaurants, no seating	1 space for each 300 sq. ft. of gross building area

¹ See provision 3.2.3-C-2, guaranteed affordable housing units.

² Net building floor area shall be defined as the total area of an establishment including patios, balconies, and any outdoor areas capable of being used for the purpose of serving food or beverages, with the exception of kitchens, restrooms, offices pertaining to the establishment only, and food and beverage service or storage areas.

Blank Page

4.0 DESIGN RECOMMENDATIONS

4.1 INTRODUCTION AND PURPOSE

Design Recommendations as part of this Specific Plan are intended to inform and guide property development and redevelopment in the North 101 Corridor Specific Plan area.

One of the basic intents of these Design Recommendations is to encourage good design without severely restricting the building owner's preferences or the dictates of economics regarding materials and style.

Another intent of these recommendations is to provide specific direction on design to the property owner, developer, architect or designer. It is not the intent of the recommendations, however, to discourage innovative design or individual expressions. The applicant that wants to create an innovative project is encouraged to do so with the understanding that the project will be subject to discretionary City review, and may be approved or denied by the City.

A development applicant should understand that these Design Recommendations are not regulatory, such as the Development Regulations, but rather discretionary. Language will state "very strongly encouraged" or "encouraged" on items the City would like to see included in a development project and "very strongly discouraged" or "discouraged" on items the City would not like to see included in a development project. Therefore, if any discrepancies between these Design Recommendations and the Development Regulations occur, the Development Regulations take precedence.

Creation of a healthy development climate along the corridor is another key purpose of these design recommendations. As new development and rehabilitation occur under the auspices of these design recommendations, other property owners are encouraged to invest in improvements, knowing that their efforts may be matched by their neighbors.

4.2 DEVELOPMENT REQUIRING DESIGN REVIEW

All new building, landscaping, exterior alterations, signage or construction projects, whether they require any other City permit or not, are subject to design review unless exempted by Section 4.3, Exemptions from Design Review. These recommendations do not affect existing buildings which are not proposing new construction, new signage, new landscaping, or exterior alterations. Except as otherwise specified herein, the provisions of Title 23, Section 23.08 of the Encinitas Municipal Code shall apply to the area designated as the North 101 Corridor Specific Plan.

4.3 EXEMPTIONS FROM DESIGN REVIEW

When in compliance with all other city ordinances, regulations, and provisions of this Specific Plan, the following projects are exempted from all provisions of Design Review:

- a. Walls and fences of 6 feet or less in height.
- b. Construction underground, which will not leave any significant, permanent marks on the surface after completion. Utility boxes, pipes, and poles shall be considered "significant permanent marks". Street furniture shall be exempt unless part of a development proposal (i.e. sidewalk cafes) or city streetscape projects requiring discretionary approval.
- c. Maintenance work on buildings, landscaping or grounds which does not significantly alter the appearance or function of the building, landscaping, or grounds.
- d. Minor exterior alterations to buildings which do not result in an increase in floor area and are substantially consistent with the existing building design and character, as determined by the Director of Community Development.
- e. Change of message/copy of existing signs. This shall not include new signs or substantial reconstruction of existing nonconforming signs, as determined by the Director of Community Development.
- f. Interior remodeling work.
- g. The construction or landscaping of one single-family detached dwelling or an addition or accessory structure to a single-family detached dwelling.
- h. Landscape maintenance and upkeep, including minor replacement of plants, as determined by the Director of Community Development.
- i. Residential swimming pools, spas, patio covers, lath houses, decks, and balconies provided that zoning and specific plan standards are met.
- j. Temporary facilities as defined in the Uniform Building Code.
- k. Exterior repainting, providing color palette is consistent with the Specific Plan Design Recommendations.
- l. Roof maintenance and repair. Roof reconstruction or use of different materials is subject to design review as determined by the Director of Community Development.

4.4 APPEALS

Any person aggrieved by a final determination made under the authority of this Chapter may file an appeal pursuant to Chapter 1.12 of the Encinitas Municipal Code.

4.5 COMMERCIAL AND MIXED USE DESIGN RECOMMENDATIONS

4.5.1 INTRODUCTION

The North 101 Corridor Specific Plan Area has primarily developed as a small scale, eclectic commercial corridor along North Highway 101 and portions of Vulcan Avenue. The commercial development consists of mostly single business, single ownership buildings constructed individually on small lots. More than any other commercial area in the City, this corridor has accommodated artisans and craftsmen with related retail sales. The visual impression created by this area reflects the way residents feel about their community. It also reflects the diversity and scale of this beach community.

Two specific goals of the design recommendations for this area include; promotion of a village scale with pedestrian orientation and, encouragement of the patronage from North Highway 101 travelers. It is also important that the design recommendations remain sensitive to and allow for the preservation and enhancement of the family-owned and village scale businesses as well as the construction of new commercial development and pedestrian oriented improvements.

The economic vitality of the commercial portion of the North 101 Corridor Specific Plan Area reflects and affects the economics of the other commercial districts within the City. Therefore, the commercial and mixed use design recommendations in this section focus on efforts to foster superior design in order to create a quality image, encourage reinvestment and improve economic vitality while at the same time reinforcing the area's eclectic character.

New developments and enhancement of existing development will evolve in a more unique and creative manner if the recommendations are flexible and provide an array of choices that can be combined in many different ways. The following recommendations will provide direction without eliminating the creative opportunities that will ultimately determine the success of many design solutions. Rather than establishing strict and rigid recommendations, a framework of discretionary statements has been created.

4.5.2 GENERAL COMMERCIAL AND MIXED USE RECOMMENDATIONS FOR COMPATIBILITY AND CONTEXT

This section provides general design principles for commercial and mixed use projects in the North 101 Corridor Specific Plan Area. This section provides the basic concepts for the creation of good community design and quality development. These general recommendations are to be used in conjunction with other, more specific recommendations for commercial and mixed use development found in subsequent parts of this section.

A. Desirable Elements Of Project Design

The following design elements are desirable and highly encouraged:

1. Significant wall articulation (insets, pop-outs, columns, canopies, wing walls, trellises)
2. Natural wall materials including stucco, stone, brick, clapboard, and ceramic tile
3. Multi-planed roofs

4. Full roof treatments
5. Roof overhangs, arcades, plazas and outdoor dining areas
6. Traditional storefront design (See Section 4.5.7)
7. Articulated mass and bulk oriented to pedestrian/villagescale
8. Significant landscape and hardscape elements
9. Shared access driveways (for new and larger developments)
10. Landscaped and screened parking and loading
11. Pedestrian orientation and village scale

B. Undesirable Elements of Project Design

The following design elements are discouraged:

1. Large blank, flat wall surfaces
2. Unpainted concrete, slumpstone or corrugated metal
3. Highly reflective surfaces (mirror windows)
4. Metal or plastic siding
5. Large front yard setbacks accommodating parking
6. Visible outdoor storage, loading, and equipment areas
7. Disjointed parking areas and awkward circulation patterns
8. Over abundance of access driveways, or unsafe locations
9. Large commercial development across numerous lots, without pedestrian orientation

C. Building Height

Building heights should relate to adjacent development to allow compatibility between new and existing development, promote a village scale and encourage pedestrian activities.

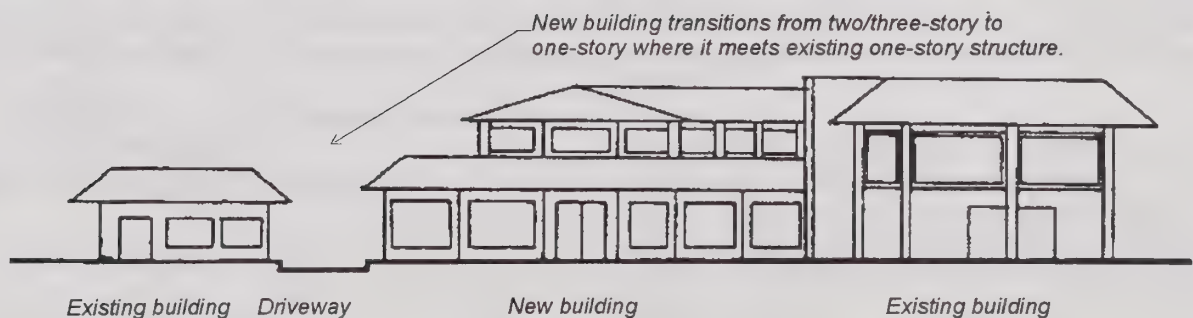
1. Varied building heights preserve the desired mix of one, two, and three-story buildings.
2. A transition from the height of adjacent development to the maximum height of the new development is highly desirable.
3. Building heights which are oriented to a pedestrian scale promote the intended "village" atmosphere.
4. Transitions from one story or two stories along the street frontage to two or three stories further back, are desirable.

D. Scale and Bulk

Scale is the relationship between a proposed building's size and the size of adjoining buildings. The scale of new buildings should be compatible with existing buildings in the area and should allow the continued domination of large trees along the North 101 Corridor Specific Plan Area.

Large scale buildings that give the appearance of square box structures are discouraged and generally will appear out of place if situated adjacent to the typically smaller buildings of the North 101 Corridor Specific Plan area. Large developments, which are constructed on three or more adjacent lots or have a frontage greater than 100' are encouraged to utilize a "village" design, such as several small buildings around a plaza area. There are several ways to reduce the appearance of excessive bulk in large buildings:

1. Vary the planes of the exterior walls in depth and/or direction.
2. Step back upper floors.
3. Vary the height of the building so that it appears to be divided into distinct elements.
4. Use varied roof planes and shapes.
5. Articulate the different parts of a building's facade by use of color, arrangement of facade elements, and/or change in materials.
6. Use landscaping and architectural detailing at the ground level to lessen the impact of large buildings.
7. Avoid new buildings with blank walls at the ground floor level. Utilize windows, wall insets, change in materials or canopies to create interest. Where blank walls are unavoidable due to storage space or other interior requirements, these walls should provide opportunities for murals, landscape backdrop, and decoration.
8. Visual relief is recommended for linear buildings by the use of architectural projections or recessed areas that create the appearance of smaller adjoining buildings.



E. Color

The following color recommendations are intended to promote building colors that are appropriate to the character and orientation of the building, and which tie together the various elements of the building in a manner which is compatible with adjacent buildings as well as the intended character of the area. These recommendations are not intended to dictate all building colors or rule out color options, but rather to provide a framework of desirable colors, and color values which are appropriate to the intended character of the North Highway 101 Corridor Specific Plan area.

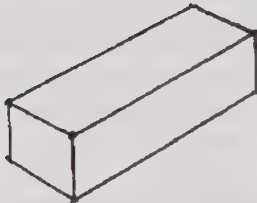
1. Light to medium intensity colors are encouraged for the overall background building color. Bright, intense colors are better reserved for accents, trim or highlighting architectural features.
2. The dominant color of new buildings should relate to the inherent color of the building's primary finish materials.
3. No limit to the number of colors used on a building is recommended. In fact, the use of numerous accent colors can be appropriate provided the color palette is compatible with these recommendations.
4. Fluorescent colors are generally not appropriate to the desired eclectic village character.
5. Color can be used to break down the scale of a building by highlighting various architectural elements.
6. Signs and awnings which are color coordinated with the building facade can successfully be used to introduce brighter or more intense colors.
7. The hues, values, and intensities of a color program should be compatible with adjoining building colors and landscaping elements, except where adjacent buildings diverge from these recommendations.
8. Large areas of intense white are discouraged.
9. Murals and supergraphics, not advertising a business, are encouraged.

4.5.3 GENERAL COMMERCIAL AND MIXED USE BUILDING DESIGN RECOMMENDATIONS

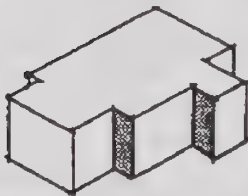
The commercial and mixed use building design recommendations for the North 101 Corridor Specific Plan area are developed to encourage new buildings and renovations to have a pedestrian scale, which is compatible with the desired village character. The following provides design recommendations for single- and multi-building commercial developments.

A. Mass and Compatibility

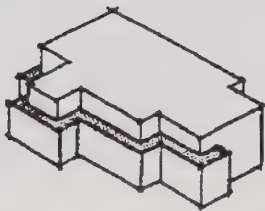
New buildings and renovations are encouraged to have pedestrian orientation and scale and to be compatible with the existing village character. The following recommendations propose ways to break up building mass and encourage compatibility between larger and smaller development.



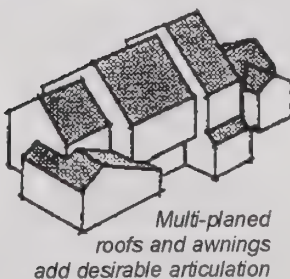
Undesirable Architectural Treatment



Vertical Articulation Added



Horizontal Articulation Added



Multi-planed roofs and awnings add desirable articulation

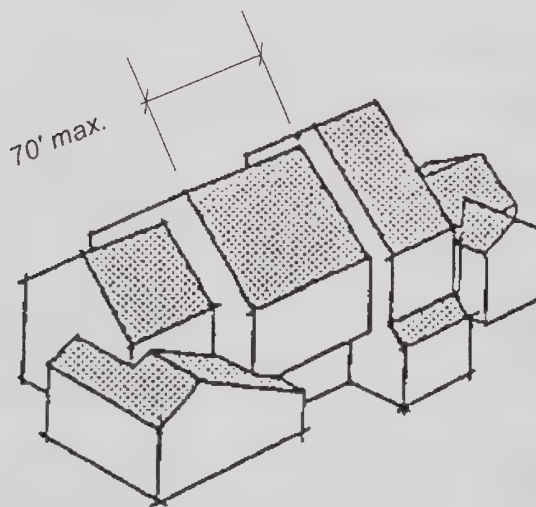
1. Breaking up large volumes or planes into smaller ones can diminish the scale of a building. Building mass can be varied in form or divided to emphasize various interior building functions.
2. Large expanses of building facades (approximately 30 feet or greater) with no architectural variations should be avoided. Building articulation and setbacks, window and entry variations, patios, plazas or other landscaped/hardscaped pedestrian areas are highly encouraged.
3. Roofline and height variations utilizing full, multi-planed roofs break up massing, provide visual interest and can be used to identify individual spaces.
4. The pedestrian scale of the street can be reinforced by distinguishing the upper floor. This can be accomplished with materials changes, window variations, overhanging building elements and other devices which focus attention on the pedestrian level.
5. Special treatment of building entrances is desirable to add interest to a facade.
6. Long, regular facades should be avoided.
7. Strong vertical elements such as windows, roofs, pilasters, columns, stairs and towers, or use of wall articulation can break up the length of a facade and create necessary variation in building style.

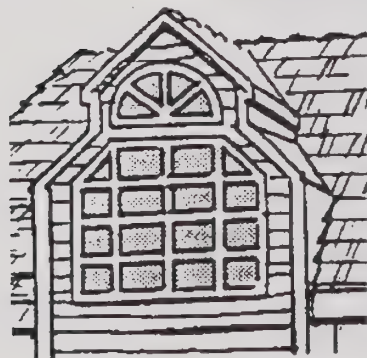
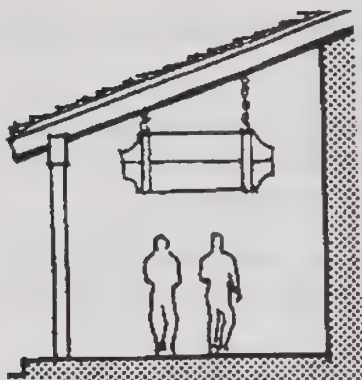
8. A diversity of scale throughout the commercial corridor encourages a village atmosphere.
9. The traditional pattern of development in the commercial corridor has been small buildings on small lots. If consolidation of lots is proposed, efforts should be made to preserve this pattern. New development using small individual buildings or building modules is strongly encouraged.
10. Additions to existing buildings should be designed to be integrated with the existing building. The new addition should match the existing building in terms of scale, window and door styles and openings, roof line, materials, color, and other aspects of design.
11. Where a newer look is desired than that found on the existing building, the entire building should be renovated to achieve a single design, except designated historic or architecturally significant buildings as determined by the Director of Community Development.
12. Terracing of buildings tends to diminish mass and provides an opportunity to conceal parking or mechanical equipment.

B. Roofs

The shape of a roof and the materials used also contribute to the overall building design, and need to be treated carefully.

1. Rooflines of large buildings (approximately 70 feet of width or more), which run in a continuous plane are discouraged. Jogging or offsetting the roofline breaks up monotony and mass and is encouraged.
2. Nearly vertical roofs (A-Frames) and piecemeal mansard roofs (used on a portion of the building perimeter only) are discouraged. Mansard roofs, if utilized on commercial structures should wrap around the entire building perimeter where feasible.





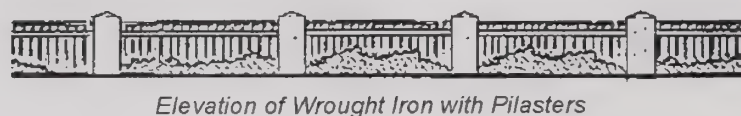
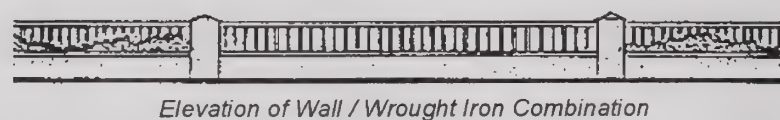
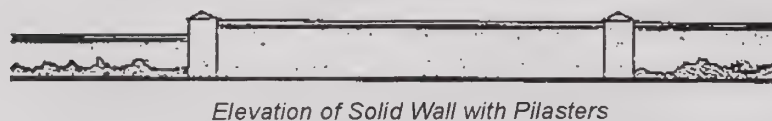
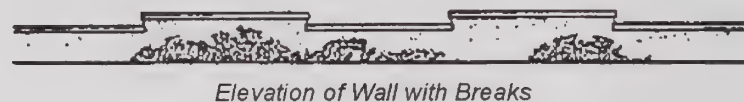
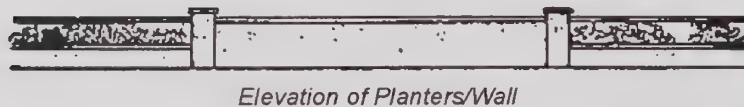
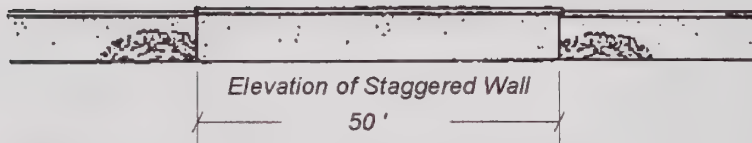
3. Roof overhangs can provide protection for shoppers from sun and rain and are encouraged.
4. Roofing materials should be consistent with the architectural character of the building and should convey a sense of permanence and quality.
5. Roof-mounted mechanical equipment must be concealed from public view by screening in a manner consistent with the character of the building.
6. Dormers, when appropriate to the character of the building, can be used to add interest to the roofline and to introduce natural light into the building.
7. The following roof materials are discouraged:
 - corrugated metal
 - high contrast or brightly colored glazed tile, except where desirable for accent purposes
 - highly reflective surfaces
 - illuminated roofing
8. Roof drains designed as an integral part of the structure and not exposed on the exterior are desirable.
9. Flat roofs are discouraged except for small freestanding buildings.

C. Lighting

1. Exterior lighting shall be compatible with the architectural style of the building.
2. Exterior lighting placement shall blend with the building's architectural style.
3. Exterior lighting should accentuate and integrate with the design qualities of the building. Exterior lighting should not overpower the visual effect of the building design.
4. Exterior lighting should maintain a moderate to low intensity level throughout the project (less than or equal to 1.5 foot candles average).

D. Walls and Fences

1. Walls viewed from the street or parking lots should be designed to blend with the site's architecture.
2. If security fencing is necessary, a design which incorporates a combination of solid walls with pillars and offsets or short solid wall segments and segments with open grill work is encouraged.
3. A wall design which provides offsets along large expanses (50 feet or more), reduces monotony. Landscape pockets along such walls or fences are also encouraged.
4. Walls and fences between buildings and the main street frontage are generally discouraged.



E. Screening Of Storage and Equipment Areas

Outdoor storage and trash areas present a negative visual experience, especially in commercial areas, and should be concealed from public view to the greatest degree possible.

1. Any exterior storage or trash area should be confined to portions of the site least visible to public view. New construction shall provide trash storage in a trash enclosure, compatible with the principal building architecture.
2. Screening should consist of a combination of elements including solid masonry walls, berms, and landscaping. Chainlink fencing with slatting is not recommended.
3. Any equipment, whether on the roof, side of building, or ground, should be screened. The method of screening should be architecturally integrated with the building in terms of materials, color, shape, and size.
4. Where individual equipment is provided reasonably close together, a continuous screen is desirable as opposed to a number of individual screens.



Unscreened rooftop equipment and utilities are prohibited.



Screened rooftop equipment and screened utility meters.

F. Building Materials (New)

The use of high quality building materials is encouraged.

1. Natural materials, such as stone, brick, tile, and wood are appropriate. Simulated or painted brick, stone, tile, corrugated metal, or wood must be carefully reviewed for appropriateness and appearance.
2. Stucco with a variety of finishes is encouraged; however, the finish should be compatible with the building character.
3. Aggregate panels, metal, or bonded metal panels and similar materials are discouraged.
4. Clear or lightly tinted glass and glass blocks can be used, but highly reflective glass is discouraged.
5. Roofing materials should be selected based on compatibility with the architecture of the building.
6. Materials can be combined to enrich the appearance of a building and highlight architectural elements; however, only a limited number of materials should be used.
7. The following materials are strongly discouraged as primary exterior wall materials:
 - unfinished concrete and concrete block
 - corrugated metal or plastic
 - reflective mirror-type glass
 - standing seam metal walls
 - plywood (painted or otherwise)
 - imitation "rock work" veneers
 - corrugated fiberglass
 - asphalt shingles
 - illuminated sidings and awnings
 - plastic laminate
 - unmilled, bare aluminum

G. Building Materials (Renovation)

1. Original exterior materials such as brick, stucco, stone, adobe, tile, vitrolite or carrera glass, wood, and metal should be retained and repaired where appropriate.
2. Original facades should be repaired only with materials that match the original materials.
3. Inappropriate materials and elements that may have been added in an earlier remodeling should be removed. These include aluminum siding, plywood false fronts, or natural, clear, or unpainted wood siding.

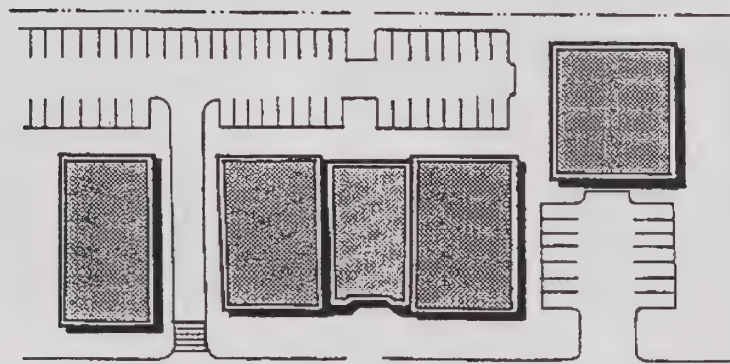
4. Deteriorated architectural features should be repaired or replaced to match the original as closely as possible.
5. Hardware that detracts from the building's appearance should be removed. This includes old sign supports, conduit, wires, and brackets.

4.5.4 GENERAL SITE PLANNING RECOMMENDATIONS

Placement of commercial and mixed use buildings, parking and pedestrian amenities is important in the North 101 Corridor Specific Plan area to encourage the desired pedestrian/ village scale. The following recommendations seek to provide design techniques which will allow a compatible mix of development size and land uses. The recommendations are aimed at promoting a pedestrian/village scale commercial corridor utilizing sensitive and innovative design.

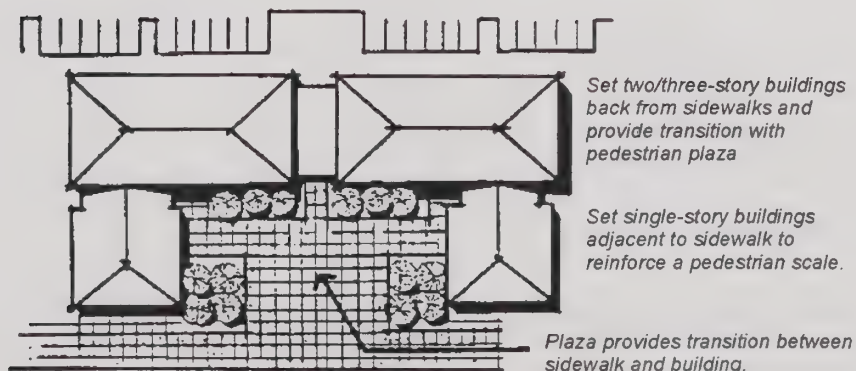
A. Setbacks and Building Orientation

1. Orientation of new buildings should consider adjacent buildings.
2. Buildings which are immediately adjacent to the sidewalk promote visibility and pedestrian orientation. This can also be accomplished if buildings are separated from the street by plazas, outside dining or other pedestrian areas.



Locate front of building adjacent to sidewalk with parking at rear.

Do not set building back from sidewalk with parking in front.



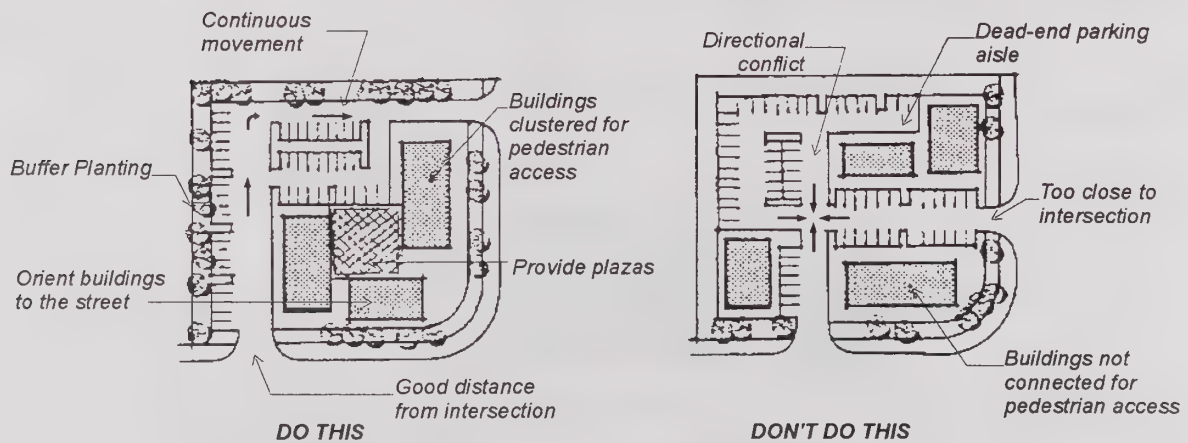
Set two/three-story buildings back from sidewalks and provide transition with pedestrian plaza

Set single-story buildings adjacent to sidewalk to reinforce a pedestrian scale.

Plaza provides transition between sidewalk and building.

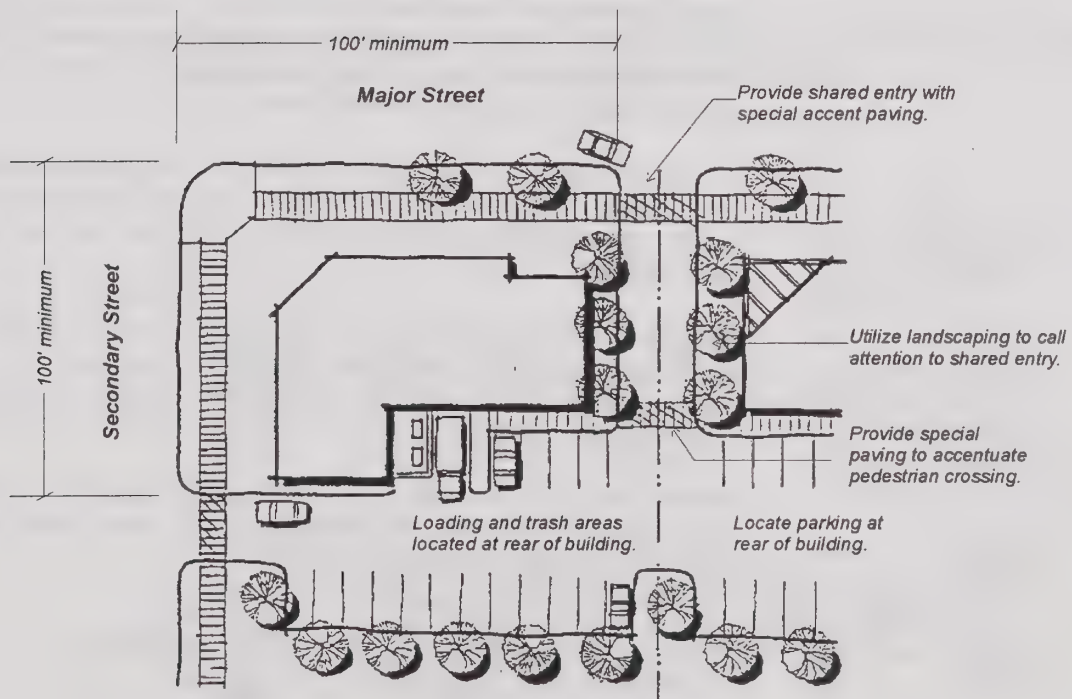
4.0 Design Recommendations

3. Larger developments which utilize more than one lot can maintain a village scale by constructing several smaller buildings rather than one large structure, and clustering the development. This creates opportunities for plazas and pedestrian areas and prevents large expanses of buildings. When clustering is impractical, a visual link between buildings can be established through the use of an arcade system, trellis or other open structure.
4. Locating buildings and on-site circulation systems such that pedestrian/vehicle conflicts are minimized encourages pedestrian activity.
5. Recognizing the importance of spaces between buildings as outdoor rooms allows these spaces to have a planned and useful shape and not simply be left over areas.



B. Street Orientation

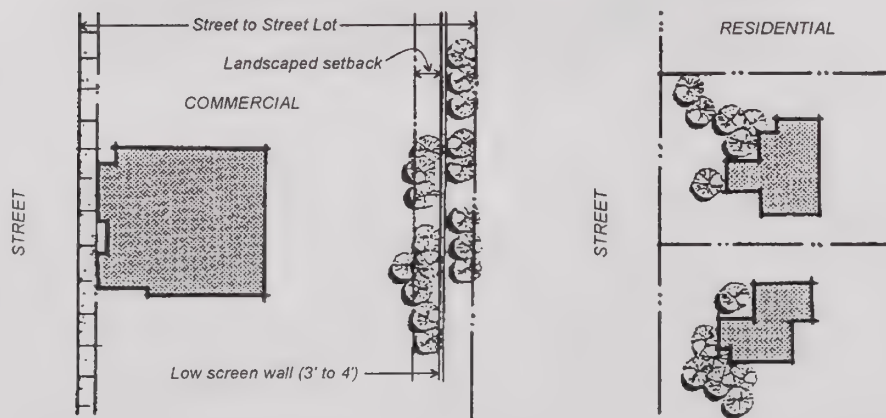
1. The building wall facing the street should generally be oriented parallel to the street.
2. Storefronts and the major pedestrian access for all buildings shall be oriented to the major street upon which it is located. Secondary rear or side entries are encouraged.
3. Buildings with parking lots between the street and structure do not promote the village scale or pedestrian activities and are discouraged.



C. Rear Yard, Alley and Street Treatments

Those commercial areas which back up to or are otherwise adjacent to residential areas are best buffered by providing landscape areas and/or screen walls along the adjacent property line. Low scale commercial buildings which are oriented to the front of the site allow side and rear portions of the site to be utilized for parking, landscaping, plazas, or other buffering amenities.

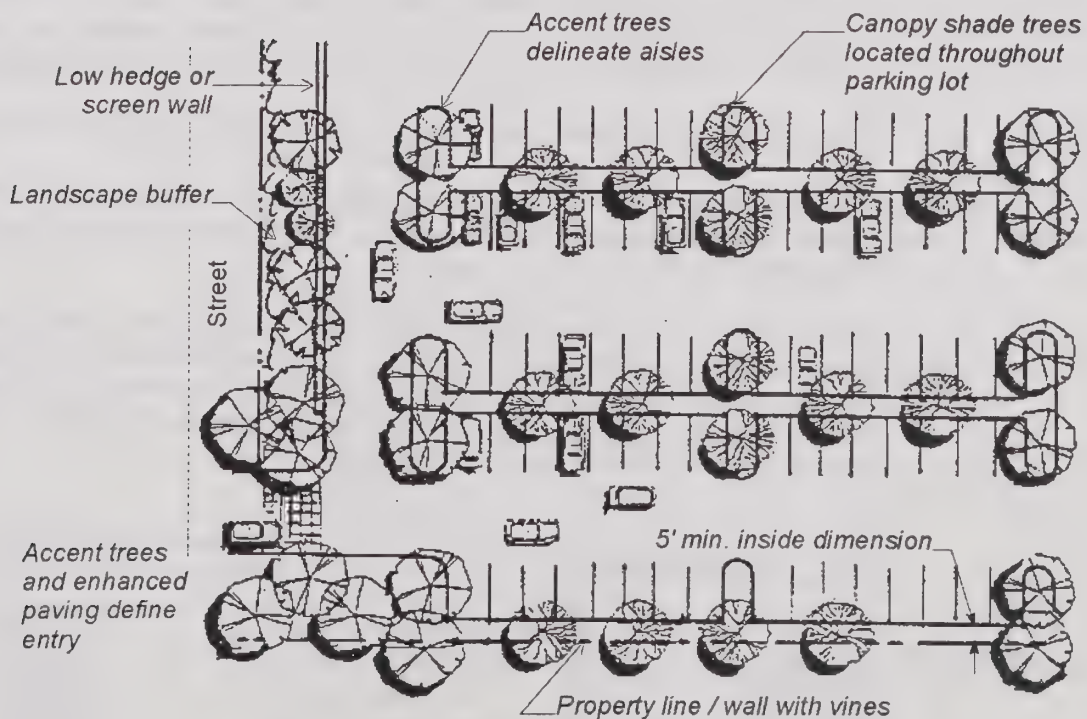
1. Those lots along North Highway 101 which have through access to Melrose and La Veta Avenues shall refer to Section 3.1.3.A & B, Development Standards, for site design requirements and Section 4.5.8.E for design recommendations.
2. Commercial or mixed use properties which are immediately adjacent to or separated by streets or alleys from residentially zoned properties can best buffer the commercial activities by providing a landscaped setback along the length of the adjacent property line. Buildings are required to be oriented as closely as possible toward the major street frontage. In some cases screen walls may be necessary to screen parking or loading areas, or unsightly building views.
3. A combination of low or opaque walls and landscape methods are encouraged for visual variety and to soften blank wall surfaces, however, pedestrian access should be provided through landscape buffer areas.
4. Consideration should be given to shared access drives for adjacent properties, both from alleys and from North Highway 101 or Vulcan Avenue. This consideration will help minimize curb cuts and reduce alley noise and traffic.
5. Where rears of buildings are visible across alleys or streets from residential lots, architectural treatment which softens blank rear walls, loading and trash areas is necessary.



4.5.5 GENERAL CIRCULATION RECOMMENDATIONS

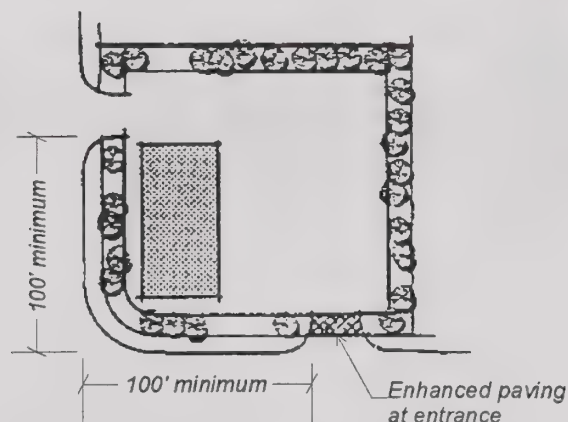
Access and parking design of commercial and mixed use properties in the North 101 Corridor Specific Plan Area is essential to ensure successful site design. The following recommendations address the important elements of access and parking design.

- A. Parking located on one side and/or at the rear of a building reduces pedestrian/vehicle conflicts and promotes the desired village scale and pedestrian activities along the North Highway 101 and Vulcan Avenue edge.
- B. On-site parking located adjacent to the street should be avoided where possible. Where on-site parking is located adjacent to the street, landscaping should be planted along the street frontage to provide needed screening. This planting should not block building visibility from the street. Hedges, shrubs, low berms, mounding, or low fences along pedestrian ways and streets should be used to effectively screen views of parked cars.
- C. Within parking areas, trees, shrubs and ground cover should be used to break up large expanses of paving, provide shade and reduce glare.

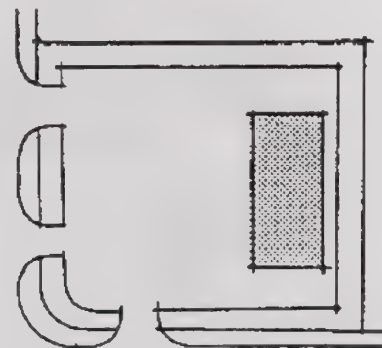


4.0 Design Recommendations

- D. Any new parking lots and structures should be located at the rear of the lot and should be softened by landscape and architectural techniques such as raised planters, etc.
- E. Parking areas should be separated from buildings by either a raised concrete walkway or landscaped strip, preferably both. Parking spaces which directly abut the building are strongly discouraged.
- F. Common driveways which provide vehicular access to more than one site are encouraged. This will effectively reduce the number of curb cuts along North Highway 101.
- G. Shared parking between adjacent businesses and/or developments shall be required where practical.
- H. Where parking areas are connected, interior circulation should allow for a similar direction of travel to reduce conflict.
- I. Parking garages and tuck-under parking located in front of a building or visible from a front or side street can diminish a viable pedestrian environment and are discouraged except where innovative design methods are used.
- J. Loading facilities should generally be located at the rear of the building and should be appropriately screened from all views (See Section 3.1.3.A & B, Development Standards for requirements for properties adjacent to Melrose Avenue and La Veta Avenue and Section 4.5.8.E for design recommendations).
- K. Parking areas shall be well lit and obstructions to visibility should be minimized.
- L. Parking access points, whether located on front or side streets should be located at least 100 feet from street intersections. Unsafe and substandard curb cuts should be eliminated.
- M. Parking areas should be designed so as to link the buildings they serve to adjacent street sidewalk or other pedestrian systems, and to give the impression of buildings as an extension of the pedestrian environment. This can be accomplished by using design features such as walkways with enhanced paving, trellis structures, and/or landscaping treatment.



DO THIS



DON'T DO THIS

4.5.6 GENERAL SIGN RECOMMENDATIONS

The following recommendations offer a variety of sign types and sizes. All new developments and renovations are encouraged to develop a sign program which integrates the development, addresses a variety of orientations and does not create a monotony of single size and similar color signs across the entire frontage. Signs should promote the desired village scale and pedestrian orientation, while providing sufficient building identification. Proposing a mixture of sign types, sizes and locations based on storefront styles, sizes and orientations can achieve the desired effect. The intent of the following recommendations is to encourage creativity and a diversity of signs throughout the North 101 Corridor Specific Plan Area. When consideration is given to the size, design and location, signs can contribute to the village atmosphere, serve as symbols of quality for commercial establishments and relate to pedestrians and traffic within the area.

A. Preferred Sign Types

1. Under canopy signs
2. Business wall mounted ID signs in sign board area at upper portion of first story
3. Awning signs
4. Blade or projecting signs
5. Building address numbers
6. Permanent and temporary painted window signs, limited to 20% of the window area
7. Building signs containing the name of the business at rear entrances used by customers

B. Discouraged Sign Types

1. Freestanding signs, except temporary A-frame signs (Freestanding signs may be acceptable for a large development site with a lengthy street frontage, and where the freestanding sign is architecturally compatible with the building)
2. Roof mounted signs
3. Animated signs, except time and temperature displays
4. Rotating, moving, swinging or flashing signs
5. Light bulb strings-except holiday decorations
6. Off-site signs
7. Paper, cloth or plastic streamers and bunting- except holiday decorations
8. Portable signs, except A-frame signs
9. Statues used for advertising
10. Traffic sign replicas
11. Signs attached to vehicles parked to advertise a nearby business
12. Inflatable signs
13. Formed plastic or injection molded plastic signs

C. General Sign Design Recommendations

The following recommendations apply to all new signs in the North 101 Corridor Specific Plan Area.

1. Use a brief message. The fewer the words, the more effective the sign. A sign with a brief, succinct message is simpler and faster to read, looks cleaner and is more attractive.

The image shows the letters 'F' and 'M' in a large, bold, simple serif font. The 'F' is positioned above the 'M'. Both letters have a clean, unadorned design with clear serifs.

***Utilize simple serif
or san serif alphabet***

2. Avoid hard-to-read, overly intricate typefaces because they reduce the sign's ability to communicate.

The image shows the words 'HARD TO READ' in a highly decorative, blackletter-style font. The letters are very ornate with many flourishes and are arranged in two lines: 'HARD TO' on top and 'READ' on the bottom. The font is difficult to read at a glance.

***Discourage typefaces
which are hard to read***

3. Carefully select color and material combinations. Colors should be selected which contribute to legibility and design integrity
4. Use a significant color contrast between the background and the message. If there is little contrast between the brightness or hue of the message of a sign and its background, it will be difficult to read.
5. Limit colors to three on a single sign. Too many colors overwhelm the basic function of communication.
6. Signs should be placed at or near the entrance to a building to indicate the most direct access to the business.

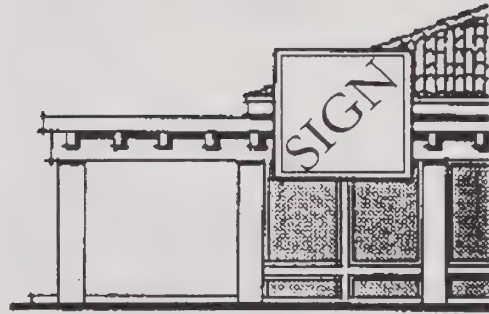
7. The size and shape of a sign should be proportionate with the scale of the structure.
8. The placement of signs should be consistent with the proportion and scale of building elements within the facade. Within a building facade, the sign may be placed in different areas. A particular sign may fit well on a plain wall area, but would overpower the finer scale and proportion of the lower storefront. A sign appropriate near the building entry may look tiny and out of place above the ground level.

DO THIS



Sign is in scale and character with building.

DON'T DO THIS



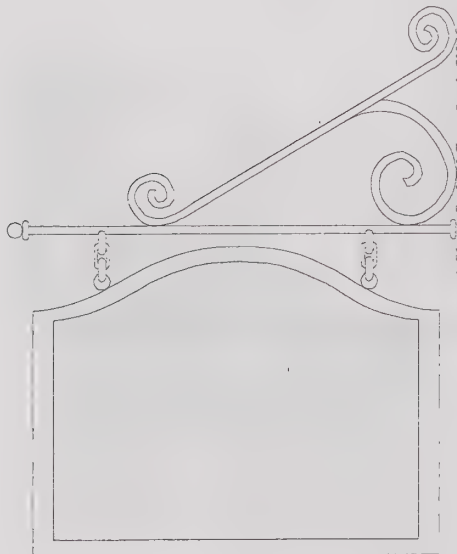
Sign is out of scale and character with building.

DELICATESSEN

Letters take up too much of the sign area.

DELICATESSEN

Letters occupy approx. 75% of the sign area (max.)

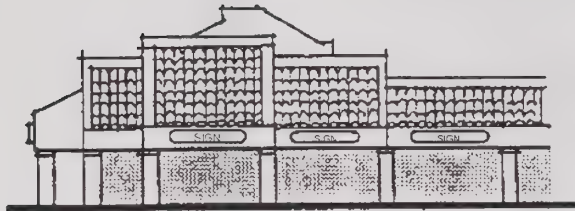


Small projecting signs help reinforce a pedestrian scale.

9. Place wall signs to establish facade rhythm, scale and proportion where facade rhythm doesn't exist. In many buildings that have a monolithic or plain facade, signs can establish or continue appropriate design rhythm, scale, and proportion.
10. Use logos rather than print/text whenever possible. Where possible widely recognized logos are encouraged.
11. Avoid signs that are unnecessarily narrow or oddly shaped which can restrict the legibility of the message. If an unusual shape is not symbolic (i.e. hammer for hardware store), it is probably confusing.
12. Carefully consider the proportion of letter area to overall sign background area. If letters take up too much sign, they may be harder to read. A general rule is that letters should not appear to occupy more than 75% of the sign panel area.
13. Limit the use of neon signs. Neon may be used as an accent feature on signs, but excessive use of neon is undesirable.
14. Make signs smaller if they are oriented to pedestrians. The pedestrian-oriented sign is usually viewed at close range and its size should be in proportion.

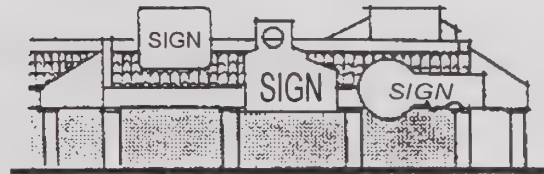
15. Design the sign to be appropriate to the building on which it is placed. The sign is an integral part of the building facade. The style of a sign should be appropriate to the style of a building's design.

DO THIS



Employ a consistent sign pattern.

DON'T DO THIS



Inconsistent sign patterns create confusion. Signs within or above roof area are prohibited.

D. Wall Mounted Sign Recommendations

1. Direct and indirect lighting methods are allowed provided they are not harsh or unnecessarily bright. The use of can-type box signs with white or light colored translucent backlit panels are not recommended on any structure in the North 101 Corridor Specific Plan area.
2. Shield external light sources. The indirect reflection of the light from the sign should be visible, rather than the light source itself.
3. Lighting intensities should be compatible with other signs and light sources.

RECOMMENDED

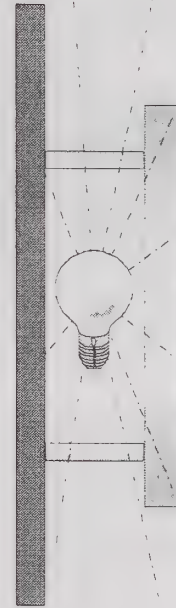


Internally lit letters with opaque background

4. Sign colors should be compatible with all other signs on that building.
5. The use of backlit individually cut letter signs is strongly encouraged.
6. For storefronts 30' wide or less, a maximum letter height of 12" is recommended.
7. For storefronts 30'-60' wide, a maximum letter height of 18" is recommended.
8. For storefronts 60' wide or greater, a maximum letter height of 24" is recommended.

E. Building Identification

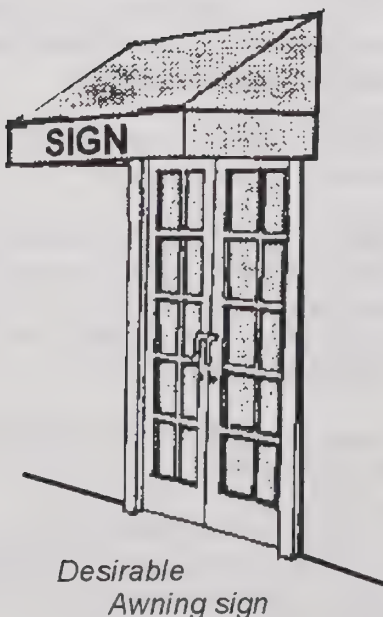
The identification of each building or store's address over the main entry doorway or within ten feet of the main entry with numbers six inches high minimum is recommended.



*Backlit individually-cut
letter signs are
strongly encouraged*

F. Permanent Window Sign Recommendations

1. When used in conjunction with wall signs, permanent window signs should not occupy more than twenty percent of the window area. The maximum height and width of these window signs should not exceed 2 feet, and total window sign area should not exceed 4 square feet. (The area of the window sign shall be included in calculating the total permanent sign area for the building but shall not be included in calculating the area permitted for temporary window signs.)
2. Permanent window signs may be used in lieu of wall signs. If so used, permanent window signs may occupy up to 50 percent of the window area but not more than the area permitted for wall signs. If permanent window signs are used, no temporary window signs are permitted.
3. Permanent window signs are limited to individual letters placed on the interior surface of the window and intended to be viewed from outside. White, black or gold leaf paint are the recommended colors. Glass-mounted graphics may be applied by silk screening or prespaced vinyl die-cut forms.
4. The text or sign copy of a window sign should be limited to the business name, proprietor's name, hours of operation, and brief messages identifying the type of product or service (e.g., "maternity wear" or "attorney") or pertinent information (e.g., "se habla Espanol" or "reservations required"). Letter sizes for all information other than the business name shall be limited to two inches in height.

**G. Awning Sign Recommendations**

1. The sign text copy is limited to the name of the business or business type.
2. The sign text should be located only on the valance of the awning and letter height must be proportionate to the valance height. Letter color should be compatible with the awning and the building color scheme.
3. The shape, design, and color of awnings shall be carefully designed to coordinate with, and not dominate, the architectural style of the building. Where other awnings are used on the building, the design and color of the sign awnings and all other awnings should be coordinated.
4. Internal Illumination of awning signs is generally not appropriate.

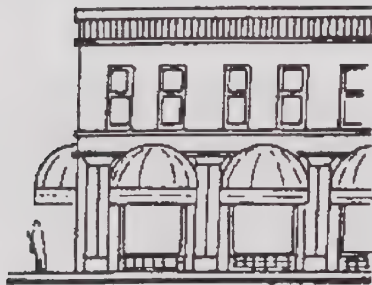
4.5.7 SPECIFIC STOREFRONT RECOMMENDATIONS

The following recommendations apply to construction of new and enhancement of existing commercial and mixed use buildings in the North 101 Corridor Specific Plan area. The recommendations for specific storefront treatments promote the village scale and desired pedestrian activities of the corridor.

A. Entries

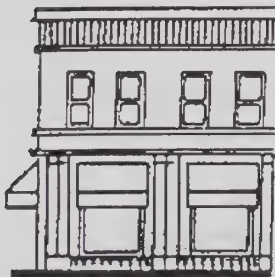
Many older storefront entries along the corridor are recessed and/or sheltered by a covered arcade or porch entry. This is an effective design because it provides more area for displays, a sheltered transition area to the interior of the store and emphasizes the entrance. Covered and recessed entries should be retained and encouraged in new construction.

DISCOURAGED



Round awning at lower level is not consistent with rectilinear building form.

ENCOURAGED



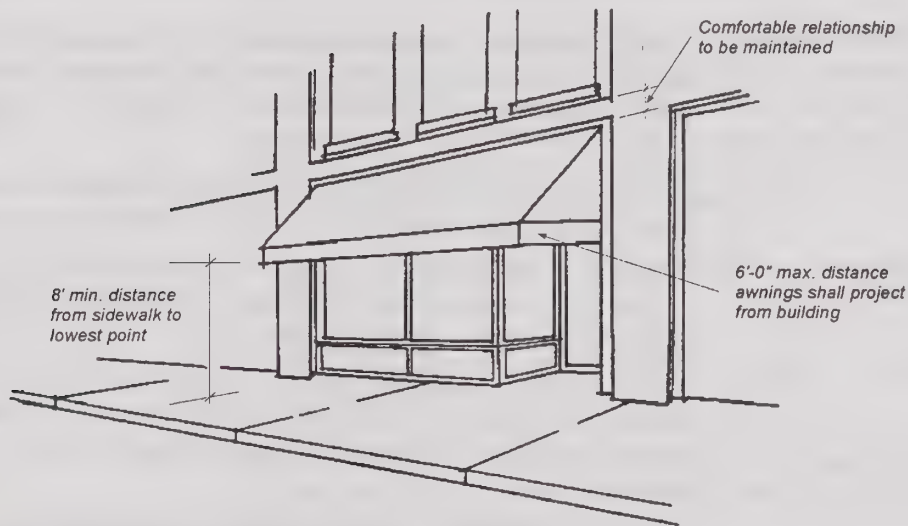
Awning in scale with facade.

B. Awnings

Awnings contribute to the village character by adding color, texture, and shade to the streetscape. It is important that awnings relate to the pattern of the building and that their size and shape is consistent with the building character.

1. Awnings should be designed consistent with the scale, proportion and rhythm created by the architectural elements of the facade.
2. Awnings by their nature are meant to be lightweight structures. Permanent "awnings" constructed with metal, tile, plexiglass, wood shakes, or similar roofing materials are discouraged.
3. Canvas, matte finish vinyl, and fabric awnings are encouraged.
4. When a building contains more than one store, different colored awnings can be used for each store when they relate visually with one another and with the building as a whole.
5. Signage placed on awnings is limited to the awning valance.
6. Minimum height of awnings should be 8'-0" from the lowest point to the sidewalk and shall not extend more than 6'-0" from the facade of the structure. (The 6'-0" dimension should be reduced when tree plantings and other obstacles dictate).

7. Awnings should have a single color or two- color stripes. Lettering and trim in other colors is appropriate.
8. When awnings are placed on two- or three-story buildings, the awning should be spaced comfortably between the architectural elements of the building's stories.
9. Care should be taken so that the awnings do not obstruct the views to adjacent businesses.
10. Awnings should be well-maintained, washed regularly, and replaced when faded or torn.



C. Doors

Storefront doors in the North 101 Corridor Specific Plan area should create a pleasant entry which invites the customer into the store. The following concepts provide recommendations for the design of storefront doors.

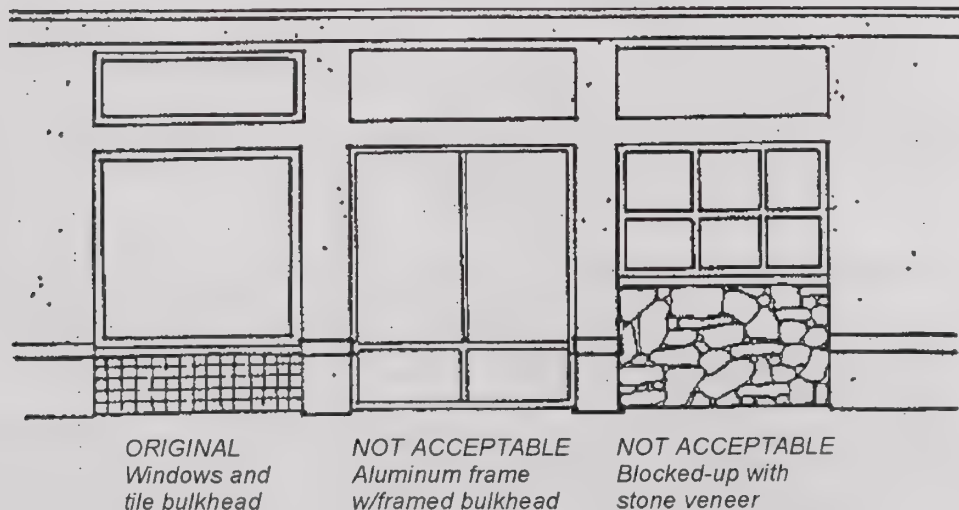
1. The door should be designed with simple details such as a handsome door pull, kickplate, or an attractive painted sign.
2. Avoid inappropriately decorated doors. Fake historic or highly decorated contemporary doors look out of place in the traditional storefront.
3. Entrances to each individual building (even when one business has expanded to include several buildings) should be retained. The rhythm of entrances is important to the sidewalk character and helps maintain a pedestrian scale.
4. Doors to retail shops should have a high percentage of glass to frame (i.e. 60-90% glass).

D. Windows

The use of windows as an architectural element is important in the North 101 Corridor Specific Plan area because windows help create the desired pedestrian orientation and village scale while providing the necessary visibility to display areas or building interiors for vehicular traffic.

The design of a particular commercial window depends as much on the nature of the business inside as the architectural style of the building. While large plate glass windows are often appropriate for larger retail stores, smaller or mullioned windows characteristic of specialty shops can also be appropriate. The following recommendations outline important considerations in developing complementary window themes for new developments (large or small) and renovations.

1. Large, continuous expanses of glass without careful detailing tend to give an undesirable urban character. Glass areas can be subdivided by mullions, panes, or decorative millwork to reduce a window to human scale.
2. To maintain a distinction between upper and lower floors, the ground level facade should have larger patterns of glass than the upper.
3. Bay, bow and box windows can be used to add visual interest to a facade by providing variety, shadow and texture. They can also become focal points for merchandising.
4. Use clear glass (88% light transmission) on the first floor. Tinted glass allowing a minimum of 50% light transmission will be considered only for use in upper floor windows. The use of reflective glass is very strongly discouraged.
5. Storefront windows should be as large as possible and no closer than 18" to the ground. By limiting the bulkhead height, the visibility to the storefront displays and retail interior is maximized. Maximum bulkhead heights for new construction should be 40". Minimum height should be 18". Existing buildings are encouraged to retrofit to these heights.



6. Replacing window and door features with incompatible materials is discouraged.
7. Air conditioning units placed in windows or over doors on the primary facade are strongly discouraged.
8. Permanent, fixed security gates, grilles or bars (defined as those clearly visible and fixed to windows or the facade) are strongly discouraged. If such bars are required they should be placed a minimum of 3 feet inside the storefront windows.

E. Storefront Accessories

1. Electronic security systems are encouraged.
2. Mechanical appurtenances attached to building facades, such as the mechanical equipment for an automatic retractable security grille, must be concealed by placement under an awning (when available) or enclosed by a housing that is in proportion with the building's architecture and painted a color, or constructed with a material, in keeping with the facade colors and materials.
3. Where transom windows exist, every effort must be made to retain this storefront feature.
4. The original bulkhead material should always be retained, maintained or uncovered, where desirable.
5. Newer storefronts can have simplified bulkheads, but bulkheads constructed of glass that simply retain a horizontal framing bar at their top are strongly discouraged.

F. Balconies

1. Balconies should be designed to be of a useable size, accessible and located to encourage use. False balconies should be used only when appropriate to the building's character.
2. The structure supporting the balcony should be designed to integrate with the building, and give the appearance of strength. The balcony should not look "tacked on".
3. The design and choice of balcony railing materials should reflect the function of the balcony and the character of the building.

G. Rear Entrances

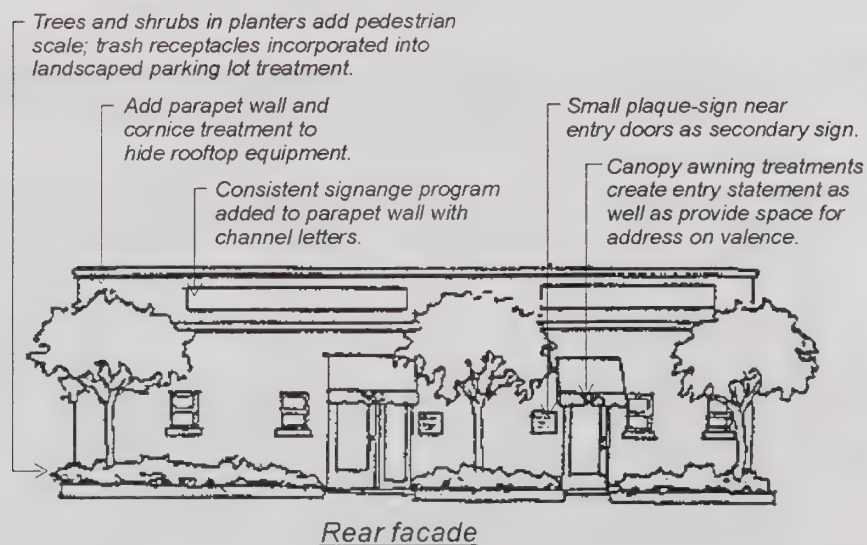
Because of the location of residential uses directly behind much of the commercial and mixed use zones in the North 101 Corridor Specific Plan area, rear facades are more visible and must be designed attractively.

In general, the rear entrance must respond to the same needs as the storefront, only at a reduced scale. These include identification signage, display, lighting, and a pleasant entry. In addition, it must also meet the service needs of the business. Since these two functions are often in conflict, the design of the rear entrance must be carefully planned. A particular concern is the storage and disposal of refuse. Trash cans, dumpsters, and other containers should be hidden from view whenever possible.

The design of a rear entrance should be appropriate to its surroundings. The design should be pleasantly inviting, but simple in detail.

1. Signs should be modestly scaled to fit the casual visual character of the alley or other rear entrance.
2. A canvas awning can soften rear facades and provide a pleasant protected space.
3. The rear entry door should be similar in design to the front door.
4. Special lighting should be modest and should focus on the entry door.
5. Selective use of tree planting and other landscaping can subtly improve a rear facade.
6. Refuse containers and service facilities shall be screened from view.

Refuse containers and service facilities should be screened from view.



H. Storefront Rehabilitation

1. Whenever a storefront is renovated, the original glass to wall ratio (typically 3 to 1) should be retained for storefronts if the original ratio is appropriate.
2. Original storefront elements should be retained and repaired, where such elements are appropriate.
3. Decorative features such as special trims, tiles, and bevelled glass should be retained and repaired, where such elements are appropriate.
4. Inappropriate materials that detract from the character of the building should be removed.
5. Any elements or signs that may be covering parts of an original storefront should be removed.
6. Original storefront windows, bulkheads, etc. should not be removed.
7. All tinted or mirrored glass should be removed.
8. Exposed air conditioning units should be removed.
9. Do not:
 - change original window size (if traditional storefront windows are desirable)
 - replace wood window and door frames with unfinished, silver aluminum
 - cover over transom windows
 - place permanent exterior security gates on windows and doors

4.5.8 DESIGN CONSIDERATIONS FOR SPECIAL USES

The design recommendations in this section cover specific uses and building types which, because of the unique design issues associated with them, require special attention. The following special recommendations supplement the more general commercial recommendations presented previously and should be considered in conjunction with them for applicable projects. Specific uses and building types covered in this section are:

- A. Hotels, motels and timeshares
- B. Automobile related uses
- C. Drive-through or drive-in businesses
- D. Mixed use
- E. Commercial or mixed use through-lots located between Highway 101 and Melrose Avenue and La Veta Avenue

A. Hotels, Motels and Timeshares

1. Design Issues

This category includes hotels, motels and timeshares, as new developments (where permitted) or renovations to existing buildings. In responding to the needs of guests, these buildings often have problems with providing sufficient amenities and services on the site while maintaining a desired level of privacy. Also, due to the inherent size of these types of uses, they can easily become imposing landmarks in relation to their surroundings, which may, or may not, become a design issue depending on the actual site location and the design quality of the structure.

2. Architectural Treatment

- a. The design and scale of the structure should relate to the desired form and pedestrian scale of surrounding structures in the North 101 Corridor Specific Plan area, but most importantly to the desired village scale for the area.
- b. Combinations of one, two or three story building elements which create sensitive transitions in form and scale with surrounding structures are preferred over linear buildings of a single consistent height. Clusters of cottage type buildings are encouraged.
- c. Whenever possible, consider how design characteristics of appropriate surrounding architecture can be repeated or adapted.

3. Materials

Use building materials and design features that promote a sense of permanence and a diversity of style and detail. More specifically, emphasize "permanent" materials (e.g. wood, brick, clapboard, stone, stucco, concrete, tile, etc.) as primary building materials. Avoid the use of glass walls, bare aluminum and plastic.

4. Site Planning

- a. Avoid locating driveways, garage ramps or loading and service areas where they interfere with the flow of pedestrian movement or impact the privacy of guest rooms.
- b. Utilize parking drives and courts or other open spaces on the site to help buffer the buildings from any adjacent incompatible land uses however, avoid locating large parking areas adjacent to North Highway 101.

5. Other Elements

- a. Small cottage type buildings are encouraged to promote the village scale of the corridor. New linear buildings with numerous adjacent room entrances opening onto parking lots should be avoided in new development.
- b. Any large developments, especially those incorporating mixed commercial uses, are encouraged to provide underground parking which is architecturally compatible and of moderate scale.



Variable roof heights and shapes, window pop-outs, balconies and planter boxes on the upper level(s) provide visual interest.

- c. Avoid exposed air conditioning units for each guest room.
- d. Screen mechanical equipment on roofs. Locate equipment in mechanical/utility rooms surrounded by a solid screening wall designed to integrate with the architecture of the building, wherever possible.

B. Automobile Related Uses

1. Design Issues

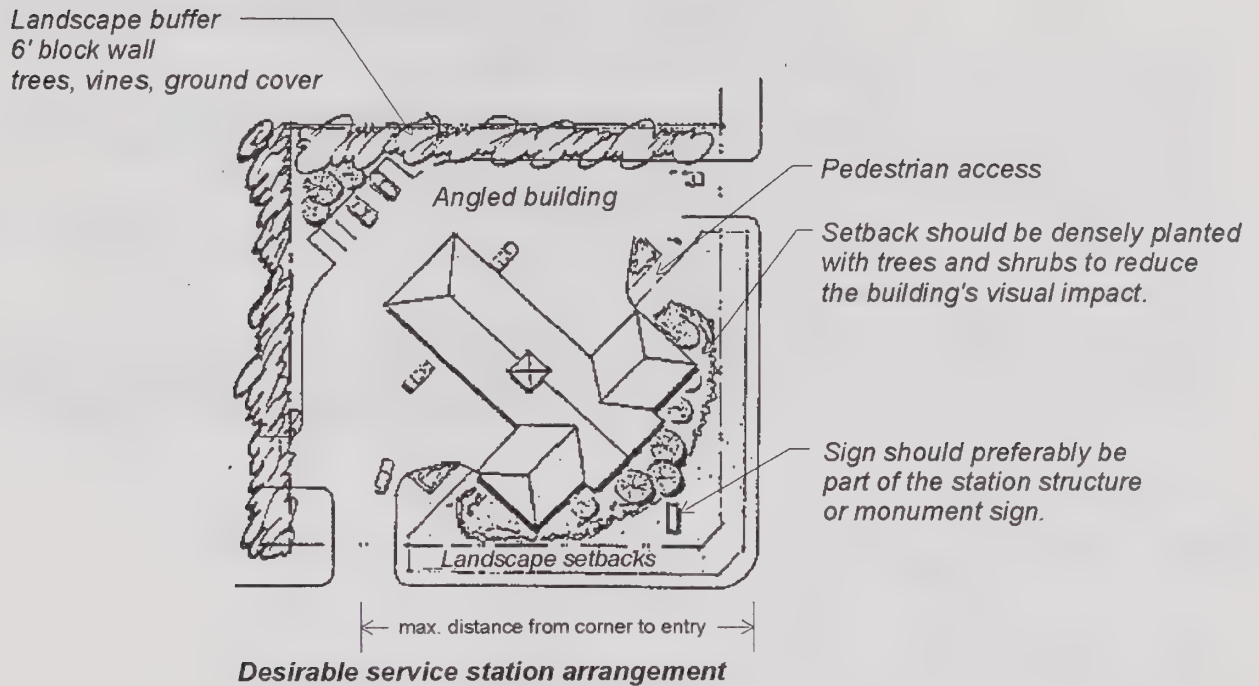
The primary design recommendations concerning automobile-related uses address the need to screen, or otherwise hide from public view, some of the more unattractive repair and storage operations. Another issue, specifically associated with gasoline service stations, is the need to accommodate site ingress and egress safely and efficiently because of the large number of turning movements these uses generate.

2. Architectural Treatment

- a. All automobile repair or work areas and facilities shall be within enclosed buildings with openings designed to eliminate noise and view impacts.
- b. Automotive use structures (including pump islands) should incorporate full roof treatments with moderate pitch, varied parapet height and complete screening of mechanical equipment.
- c. Architectural treatments should conform to the design recommendations for other commercial and mixed use structures found in this chapter.
- d. Service stations should incorporate facade material to produce texture and to provide interest. Such materials include, but are not limited to, split face block, brick, slumpstone, clapboard, textured block or stucco. Painted metal siding is discouraged.
- e. All automotive repair bays shall be provided with roll-up doors (or similar) with all operating mechanisms located on the interior of the structure.

3. Site Planning

- a. Service areas for all automotive uses should place bay door openings to the side or rear of the structure (with proper screening from residential uses) to allow landscape setbacks and more unobstructed sidewalk area. This promotes the desired pedestrian orientation and scale.
- b. Curb cuts should be located as far from intersections as possible.
- c. Curb cuts for adjacent automotive uses along North Highway 101 should be no less than 25 feet apart and should be a minimum of 30 feet wide to allow ease of ingress for fast moving traffic.



C. Drive-through or Drive-in Businesses

1. Design Issues

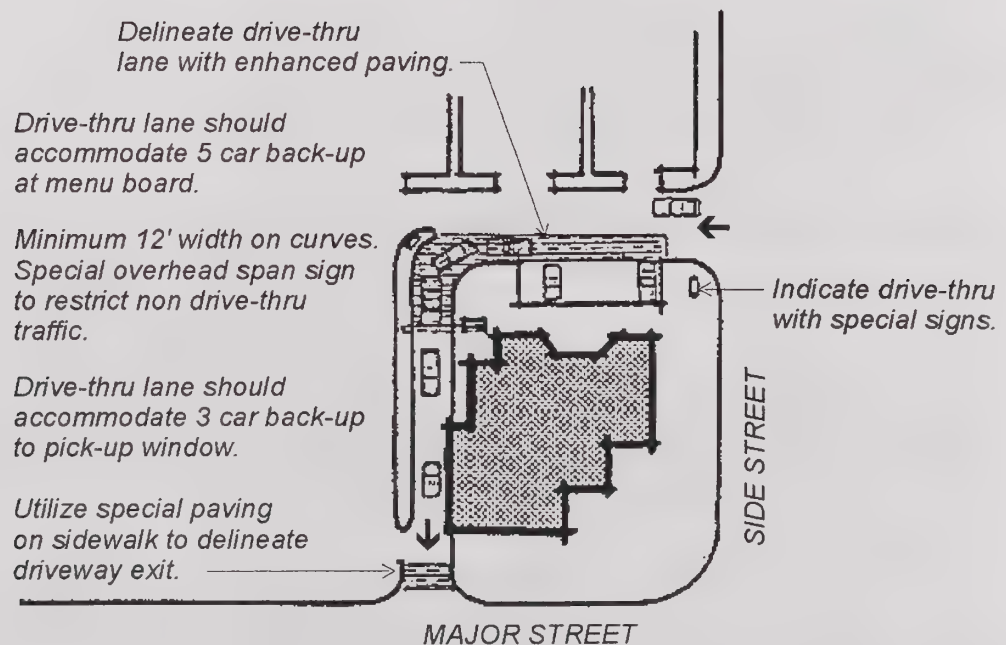
- a. This section establishes recommendations for development of businesses which rely on drive-through or drive-in patronage. Such businesses include, but are not limited to, fast food restaurants, banks, savings and loans, and car washes.
- b. The major design issues related to these types of establishments are the components of the site plan that promote efficient vehicular access and on-site circulation, while adequately buffering adjacent uses.

2. Architectural Treatment

Architectural treatments should conform to the design recommendations for other commercial and mixed use structures found in this chapter.

3. Site Planning

- a. All drive-through establishments along North Highway 101 should be located at corner locations. Mid block locations are strongly discouraged.
- b. Drive-through aisles should provide adequate on site queuing distance to accommodate a minimum of 5 cars before the first stopping point (e.g. menu board, teller window).
- c. Drive-through aisles should have a minimum 25 foot interior radius for any curves.
- d. Whenever physically possible, the main structure should be sited so as to maximize the distance for vehicle queuing while screening the drive-through operations located on the back side of the structure.



D. Mixed Use Projects**1. Design Issues**

For the purpose of these recommendations, mixed use projects are defined as developments which combine both commercial and residential uses or structures on a single lot, or as components of a single development. The uses may be combined either vertically on the site within the same structure, or spread horizontally on the site in different areas and structures.

The primary design issue related to mixed use projects is the need to successfully balance the requirements of residential uses, such as the need for privacy and security, with the needs of commercial uses for access, visibility, parking, loading, and possibly extended hours of operation.

2. Architectural Treatment

- a. Architectural treatments should conform to the design recommendations for other commercial and mixed use structures found in this chapter.
- b. The architectural style and use of materials should be consistent throughout the entire mixed use project. Differences in materials and/or architectural details should only occur on a structure where the intent is to differentiate between the residential scale and character of the structure and the commercial scale and character.
- c. The design of storefronts should be consistent with the design recommendations for commercial development. The residential portion of a mixed use structure should use design elements such as windows and balconies that reflect a residential character.
- d. All projects should incorporate full roofs where feasible.
- e. Structures with heights greater than one story should step back upper portions of the structure.
- f. All roof mounted equipment shall be screened in accordance with the requirements of this Specific Plan. Special consideration should be given to the location and screening of noise generating equipment such as refrigeration units, air conditioning, and exhaust fans. Noise reducing screens and insulation may be required where such equipment has the potential to impact residential uses.

Step back portions of the building to break up wall planes and add visual interest.

Use variable roof shapes and heights.



Commercial storefronts maintain single-story adjacent to street, while two and three-story residential units are set back.

3. Site Planning

- a. Mixed use projects that provide commercial space on the ground floor with residential units above are encouraged.
- b. Separate site access drives and parking for residential and commercial uses are encouraged where lot size and access make it feasible.
- c. Site access drives should incorporate distinctive architectural elements and landscape features which help to differentiate access to commercial parking areas from residential areas. Security gates should be considered for access to residential uses and residential parking areas, as well as for securing commercial parking areas when businesses are closed.
- d. Loading areas and refuse storage facilities should be located as far as possible from residential units and should be completely screened from view from the residential portion of the project. The location and design of trash enclosures should account for potential nuisances from odors.
- e. Parking lot lighting and security lighting for the commercial uses should be appropriately shielded so as not to spill over into the residential area or impact the residential units in any way. Residential units should also be shielded from illuminated commercial signage.

E. Commercial or Mixed Use through-lots located between North Highway 101 and Melrose Avenue and La Veta Avenue

This section deals with those commercial or mixed use through-lots located between North Highway 101 and Melrose Avenue and La Veta Avenue within the North 101 Corridor Specific Plan area.

1. Building frontages shall be oriented toward North Highway 101.
2. Buildings shall be sited as closely as practical to the front of the lot, allowing as much of a buffer area between the rear of the building and Melrose Avenue and La Veta Avenue as possible.
3. Architecturally, the rear of any buildings visible from Melrose Avenue and La Veta Avenue should have equal design treatment as the front. Storefront "rear entrances" are encouraged in lieu of simple fire exits.
4. Loading areas and doors should be located along the side of the buildings, or incorporated into decorative "rear entrance" designs or otherwise screened from view from Melrose Avenue and La Veta Avenue.
5. Trash enclosures should be incorporated into new building designs wherever possible. Enclosures should not be located within thirty feet of the Melrose Avenue and La Veta Avenue property line. Any new enclosures which are not an integral part of the building design should match the architecture of the building and should be effectively screened from view.
6. Clearly identifiable pedestrian access is encouraged off of Melrose Avenue and La Veta Avenue.
7. Vehicular access off of Melrose Avenue and La Veta Avenue is discouraged and should be avoided wherever possible.
8. Pedestrian ways through parking areas from Melrose Avenue and La Veta Avenue shall be separated from drive aisles and shall have definite design and landscape features to differentiate them from the parking areas.
9. Low screen walls and fences used in conjunction with landscape berms and mounding are encouraged to buffer buildings, and loading and parking areas.

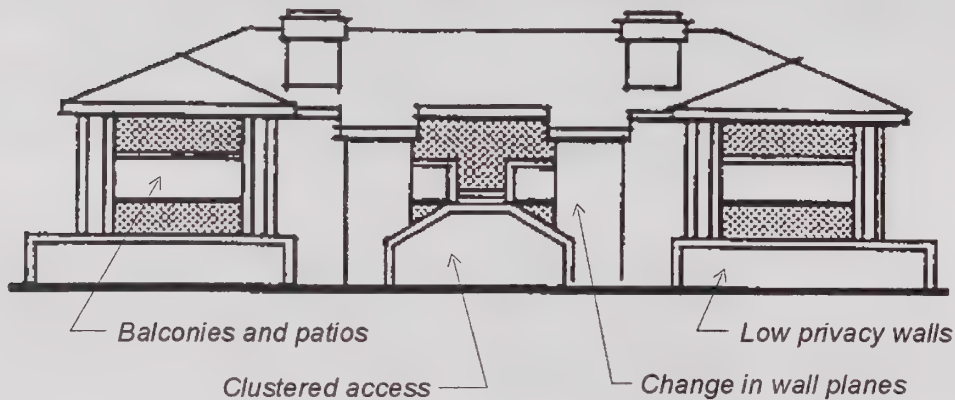
4.6 RESIDENTIAL DESIGN RECOMMENDATIONS

The intent of the following recommendations is to encourage superior design of residential projects through the appropriate use of materials and the creation of visually interesting developments.

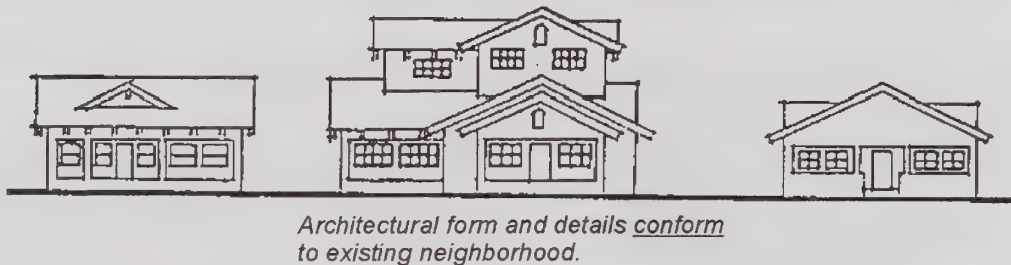
4.6.1 ARCHITECTURAL CONSIDERATIONS

- A. New residential development should be compatible with the desirable characteristics and scale of the surrounding neighborhood.

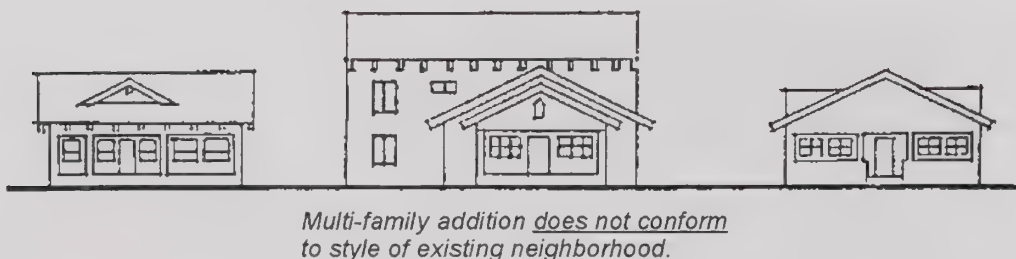
Full, low roof line reflects residential character.



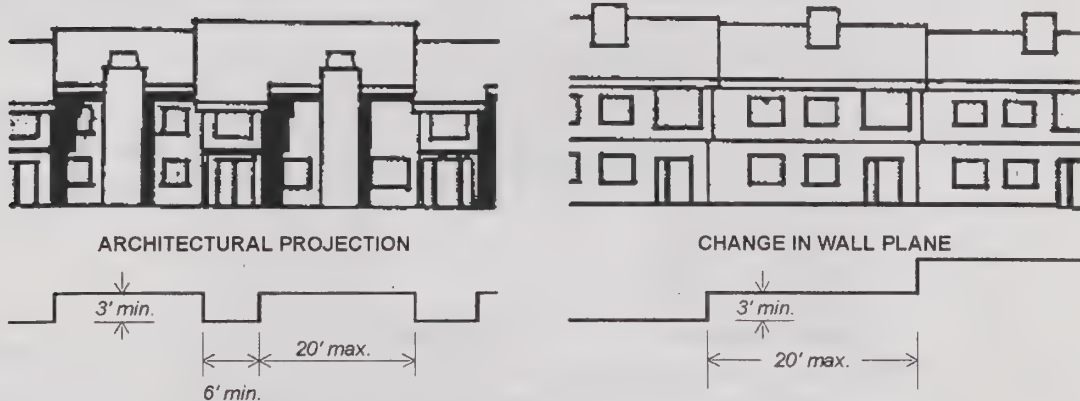
Appropriate infill of multi-family structure



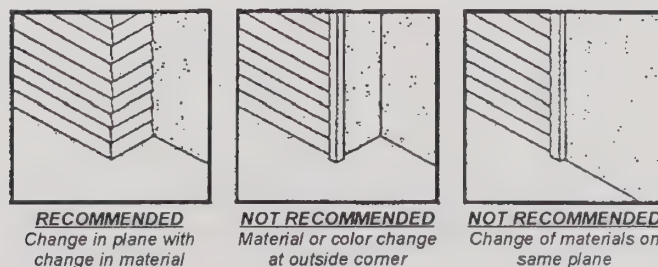
Inappropriate infill of multi-family structure



- B. Changes in plane and height, and the inclusion of elements such as balconies, porches, arcades, dormers, and cross gables are encouraged.
- C. Hipped or gabled roofs covering the entire building are strongly encouraged in lieu of mansard roofs or segments of pitched roofs applied at the building's edge.
- D. Wall planes should not run in one continuous direction for more than 20 feet without an off set of at least 3 feet. Where this is accomplished by the use of a building projection, such projection should be minimum of 6 feet wide.



- E. Major design orientation shall be onto street frontages, so that pedestrian entries, window openings, walkways, etc. are oriented onto front and side streets.
- F. Building materials should be very durable, require low maintenance, and relate a sense of permanence. Excessive changes in materials should be avoided.
- G. The selection of appropriate exterior building materials should be based on the desirable style and materials used in the surrounding neighborhood. Veneers should turn corners, avoiding exposed edges.

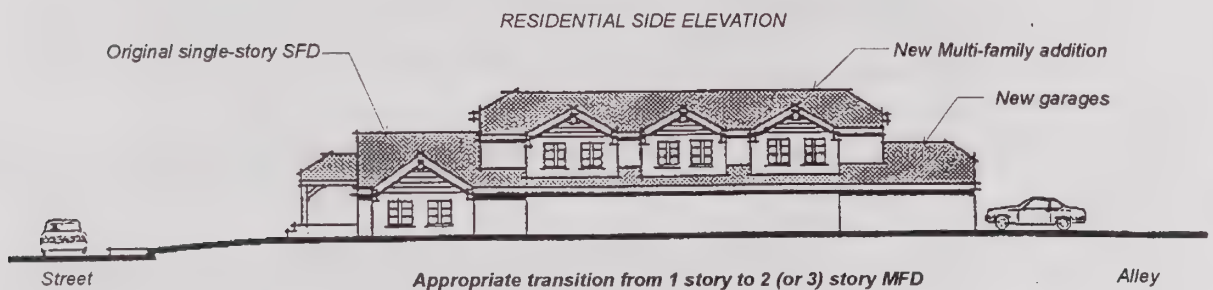


4.0 Design Recommendations

- H. Carports, detached garages, and accessory structures should be designed as an integral part of the overall project. They should be similar in materials, color, and detail to the principal buildings of a development.
- I. All mechanical equipment whether mounted on the roof or ground should be screened from view. Screen walls should be visually compatible with structures.

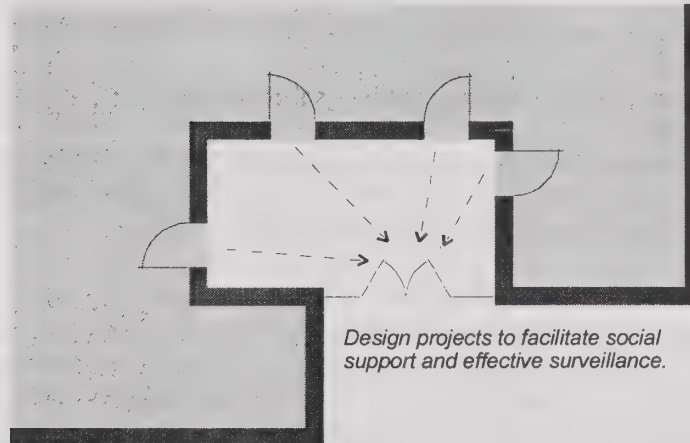
4.6.2 SITE DESIGN CONSIDERATIONS

- A. New residential developments should be designed to continue the desirable on-site relationships of the original structure(s) and surrounding neighborhood. Parking should be located in the rear and access taken from the side streets or alleys whenever possible.
- B. The height of residential projects should be considered within the context of their surroundings. Buildings with greater height may require additional setbacks at the upper stories so as not to impose on adjacent single story uses.



4.6.3 SECURITY CONSIDERATIONS

- A. It is important that all entrances to a site be designed to be easily visible whether from a public street or alley. It is also essential that interior and exterior access points and corridors be well lit.
- B. Interior open spaces, courtyards, circulation corridors and balconies, and individual living unit entrances should be visible from a majority of the living units. The units should also allow easy surveillance from within the living units.



- C. Obscured spaces where intruders might hide should be avoided in the design of all new residential projects.
- D. Lighting for pedestrian walkways should be maintained at 0.5 footcandles for areas such as through common areas, and at 0.8 footcandles for security areas such as at site entrances and walkways from parking areas.
- E. Garages or security gates should be provided for parking areas, where feasible.

4.7 STREETScape CONCEPTS

4.7.1 INTRODUCTION

The purpose of this section is to provide recommendations for the streetscape design in the North 101 Corridor Specific Plan area. The information provided in this section identifies and coordinates the streetscape design elements of paving (sidewalks and crosswalks), street trees, street furniture, lighting, median treatments, special intersection treatments, signs, and a linear park adjacent to the railroad tracks.

These recommendations will be used by the City in establishing capital improvement projects and revising streetscape standards for the area, and as a guide for informing private developers about some of their "off-site" improvement responsibilities.

All of the streetscape and right-of-way modifications identified in this section will occur within the public right-of-way and will be primarily initiated through the City's Capital Improvement Program process. However, when opportunities arise where private developments are occurring, developers may be required to install these "off-site" improvements as part of their conditions of approval.

4.7.2 APPLICABILITY

The Streetscape Concepts provided in this section apply to all applicable areas of the public right-of-way and to some extent, the railroad right-of-way within the North 101 Corridor Specific Plan Area. They provide the design criteria that the City and future developers will follow in the preparation of plans for implementation of the various elements.

4.7.3 GOALS

The Streetscape Concepts for the North 101 Corridor Specific Plan Area have been prepared with the following goals in mind:

- A. To strengthen and enhance an eclectic, scenic highway environment.
- B. To beautify the North 101 Corridor in order to improve the overall visual "image" of the Specific Plan Area.
- C. To enhance the motorist's "sense of arrival" into the North 101 Corridor Specific Plan area.
- D. To create a unique "sense of place" and identity for the North 101 Corridor Specific Plan area.
- E. To improve pedestrian safety through appropriate streetscape design.
- F. To consider the need for "long term maintenance" and durability in the selection and design of streetscape elements.

4.7.4 STREETScape DESIGN CONCEPTS

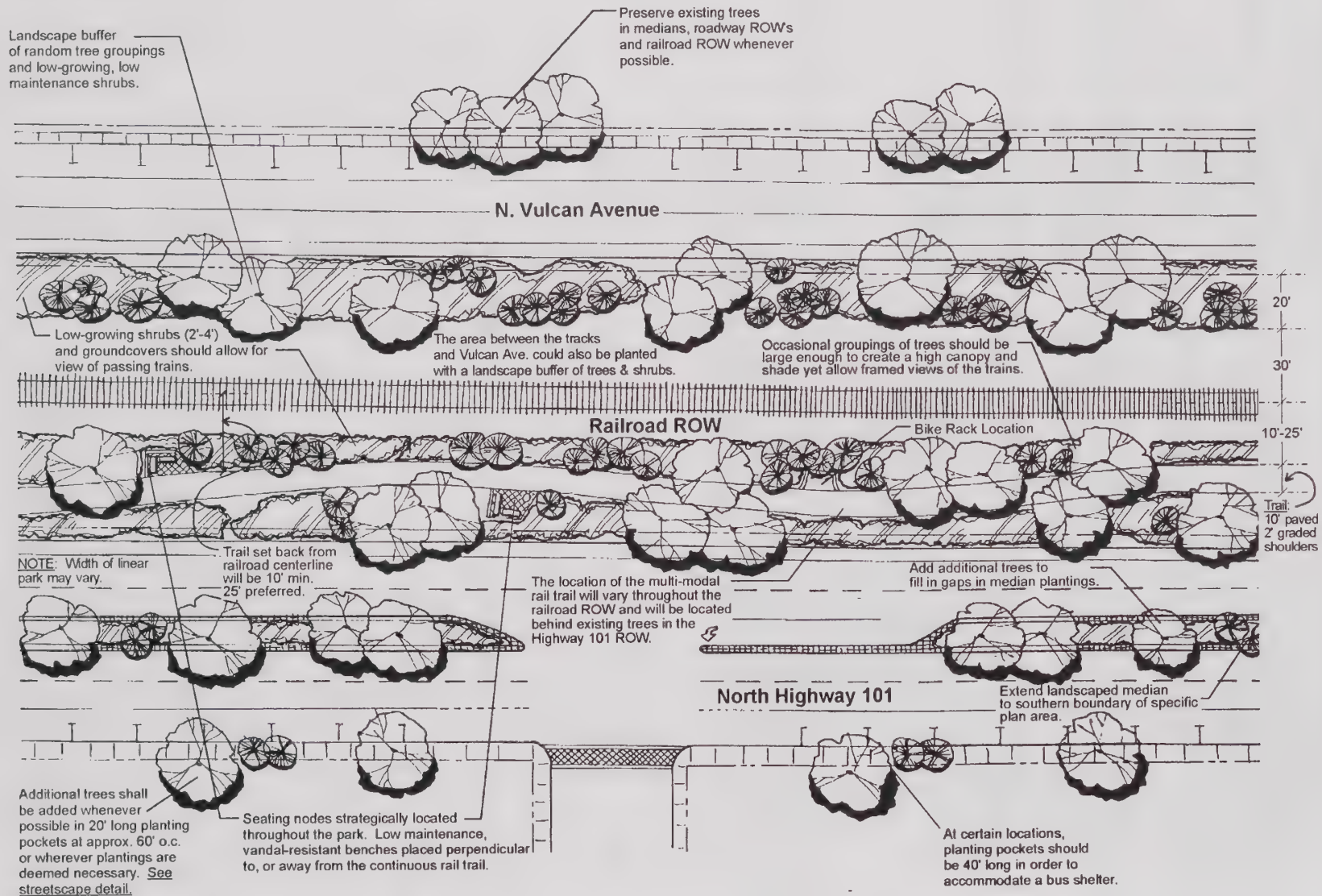
The overall general streetscape recommendations for the North 101 Corridor area shall include:

- A. Architectural Theme - selected street furniture does not have to match exactly in style or color, but should be compatible.
- B. Street Furniture - should discourage vandalism and be low in maintenance.
- C. Seating Nodes - create seating nodes at Leucadia Roadside Park and along the linear park that include benches and bike racks. Avoid seating that is open and parallel to the line of sidewalk to avoid skateboard abuse.

The following section provides details of the various design elements that make up the Streetscape Concepts for the North 101 Corridor Specific Plan Area.

- 4-A. North Highway 101 and North Vulcan Avenue Streetscape Concept
- 4-B. North Highway 101 and North Vulcan Avenue Typical Cross-Section
- 4-C. North Highway 101 Parking Alcove Concept
- 4-D. North Highway 101 Bus Turnout Concept
- 4-E. Bus Stop Area and Sidewalk Planting Pockets Concept
- 4-F. Leucadia Boulevard/North Highway 101/North Vulcan Avenue Intersection Concept
- 4-G. Leucadia Roadside Park Streetscape Concept
- 4-H. North 101 Corridor Comprehensive Streetscape Concept Plan

Figure 4-A: North Highway 101 and North Vulcan Avenue Streetscape Concept



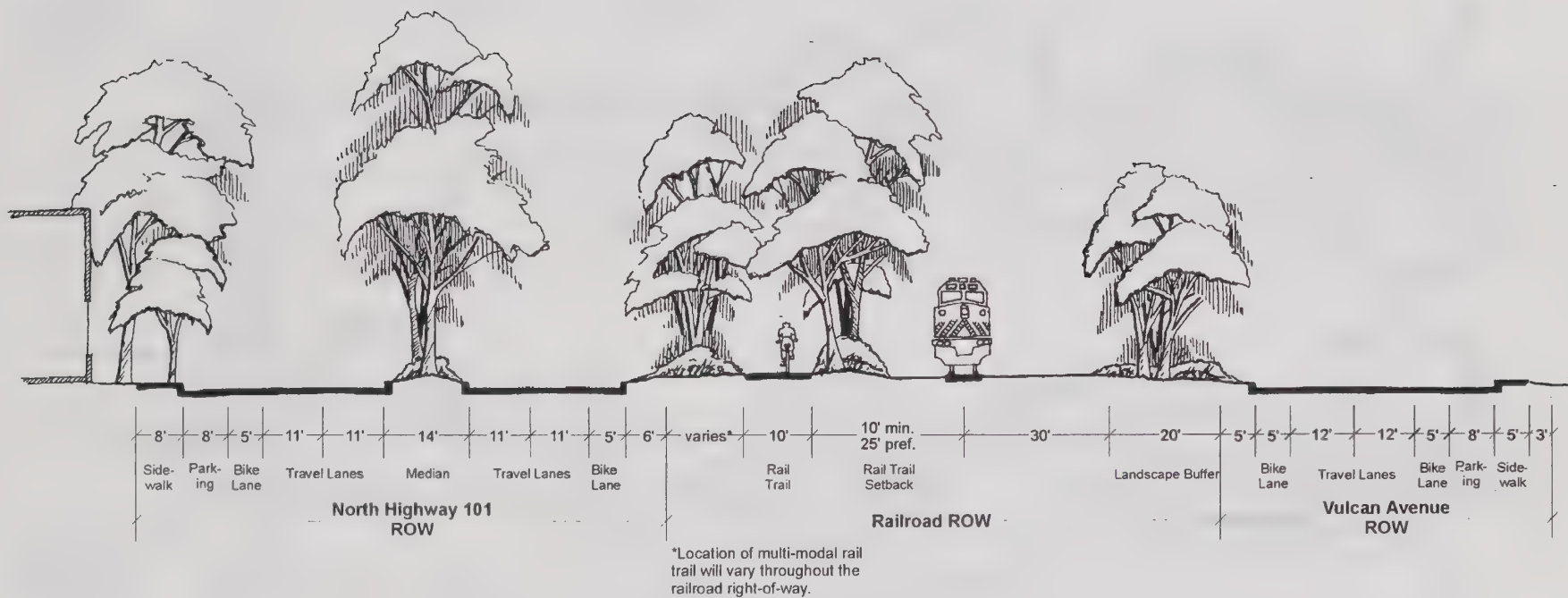
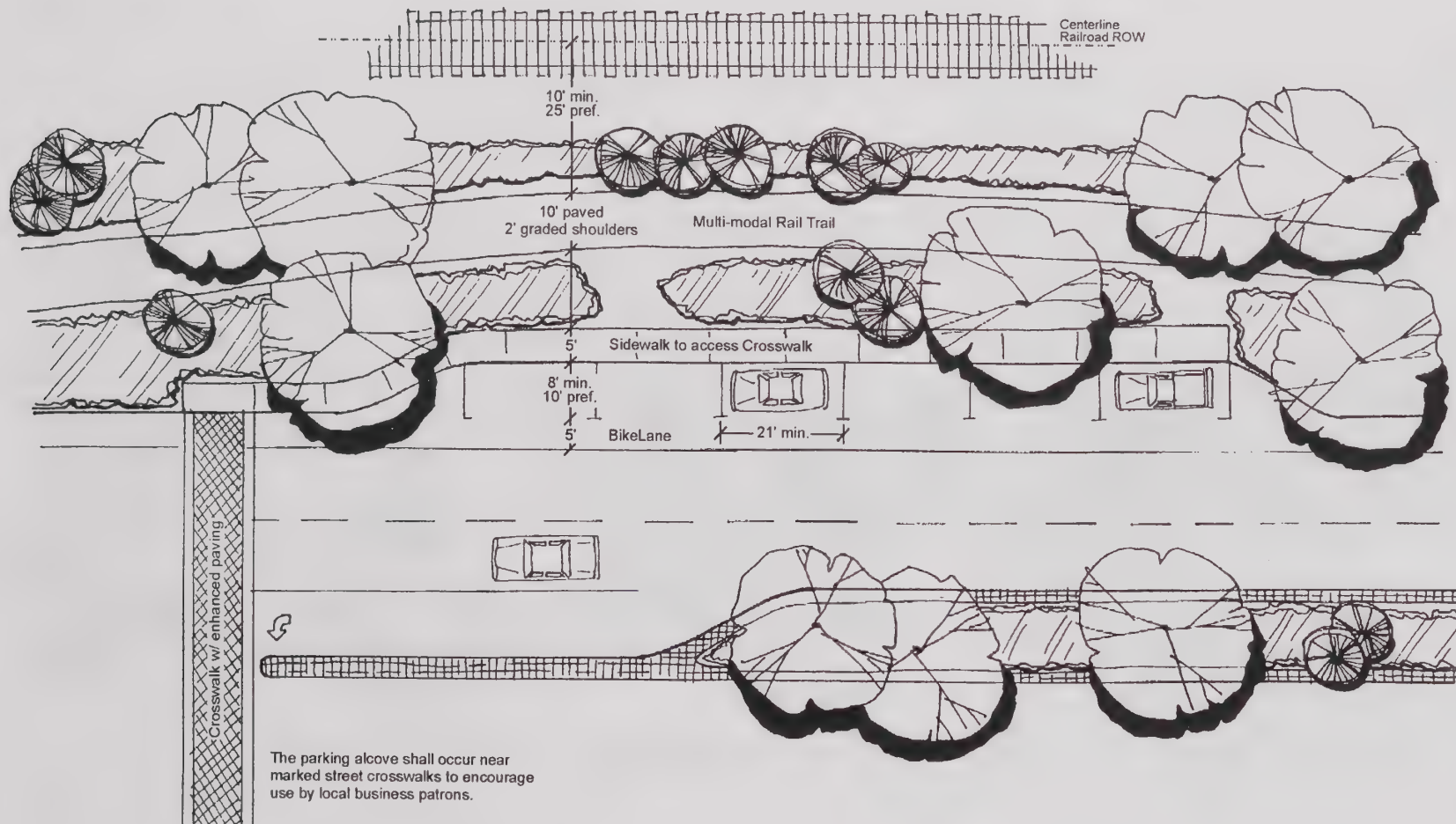


Figure 4-B: North Highway 101 and North Vulcan Avenue - Typical Cross-Section

Figure 4-C: North Highway 101 Parking Alcove Concept



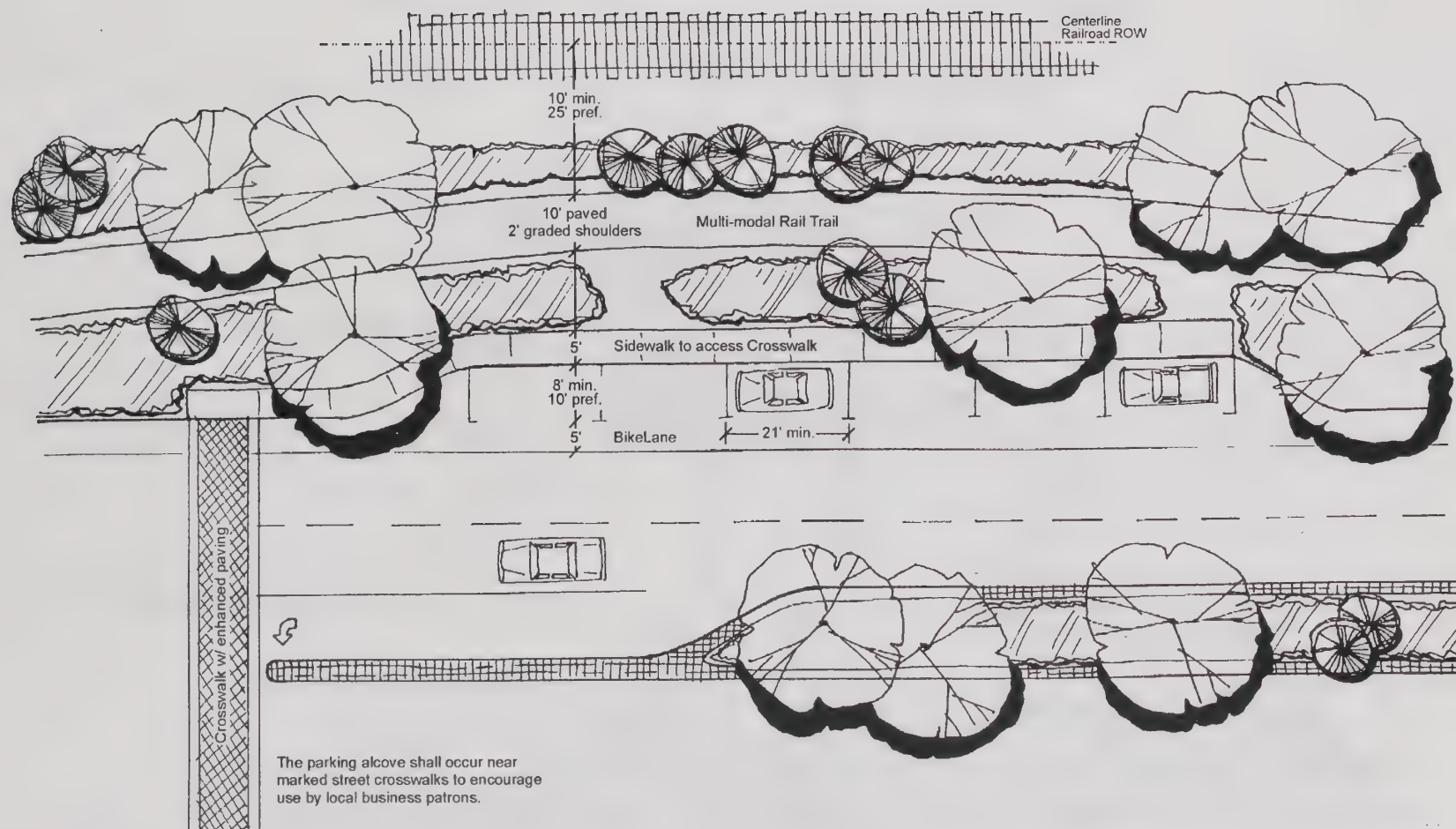
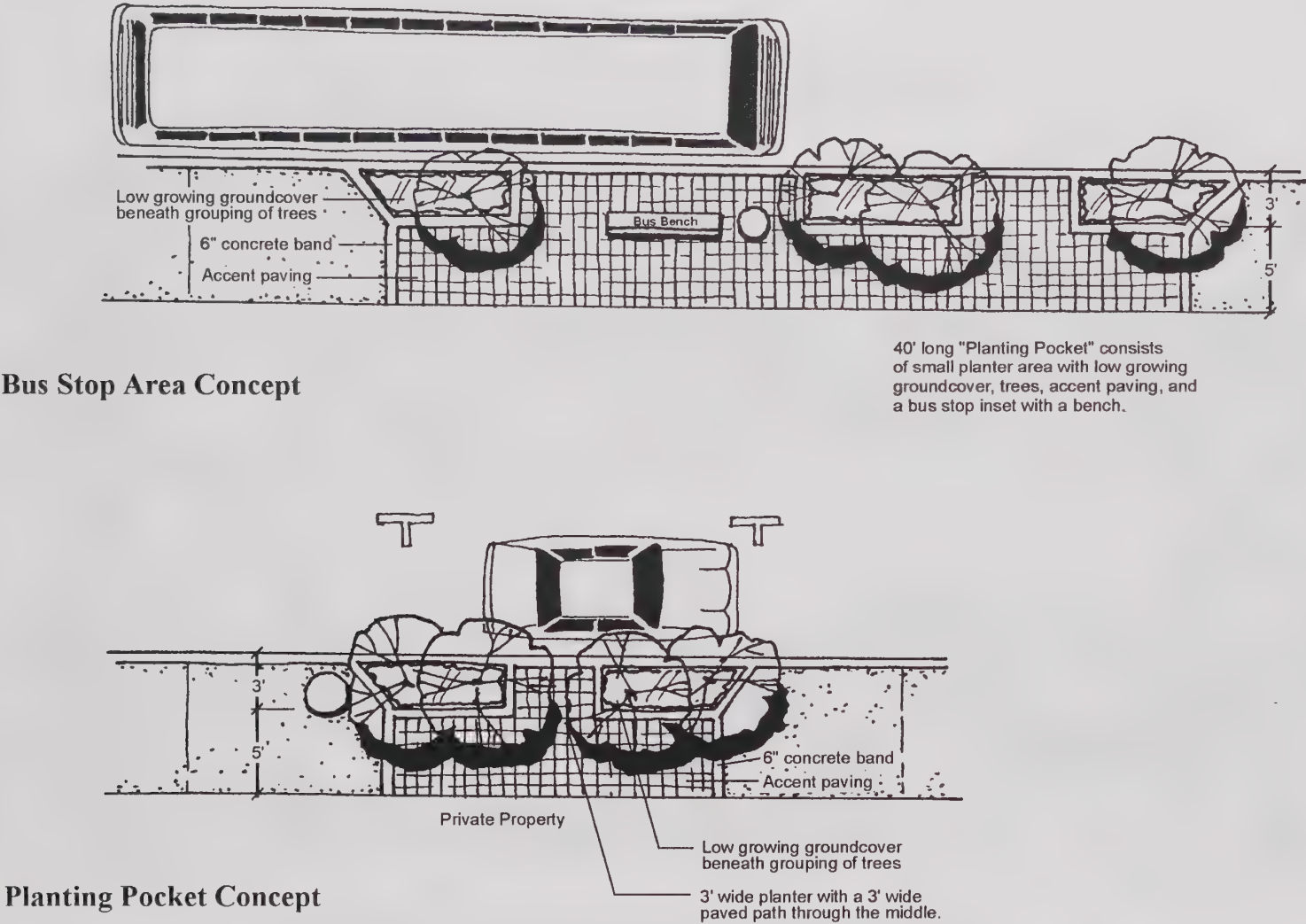


Figure 4-D: North Highway 101 Bus Turnout Concept

Figure 4-E: Bus Stop Area and Sidewalk Planting Pockets Concept



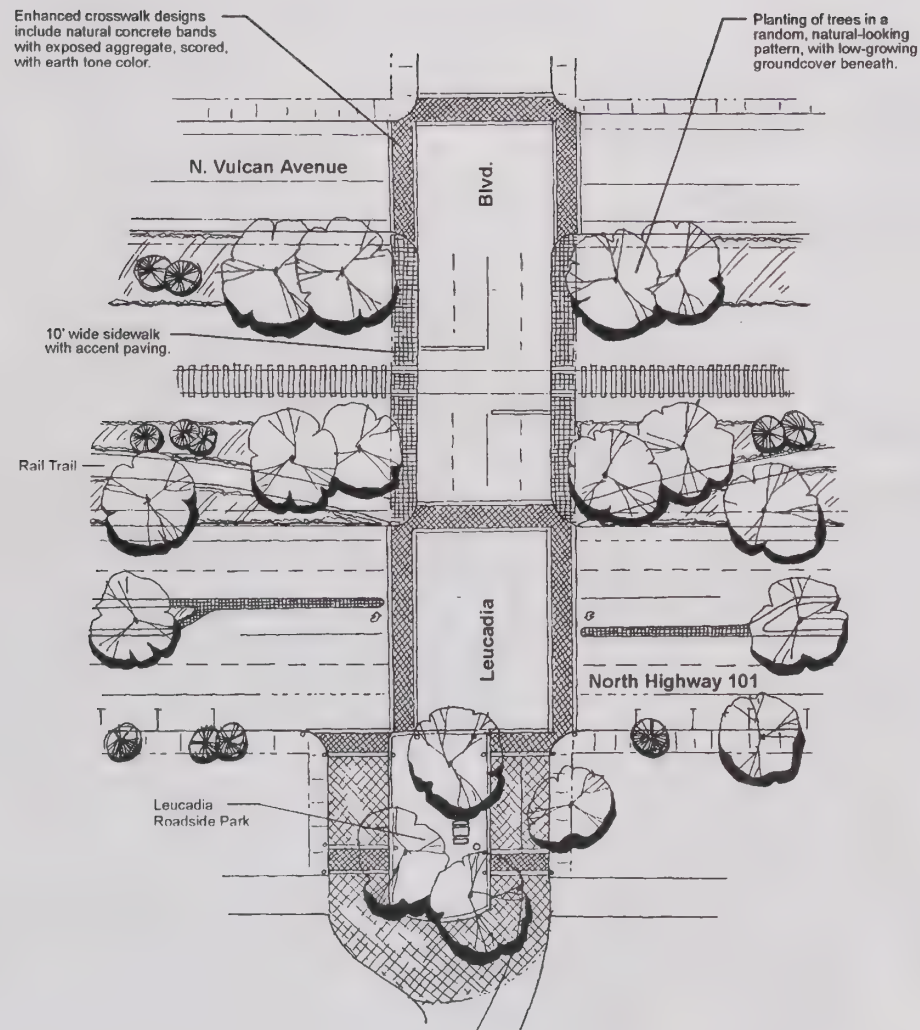
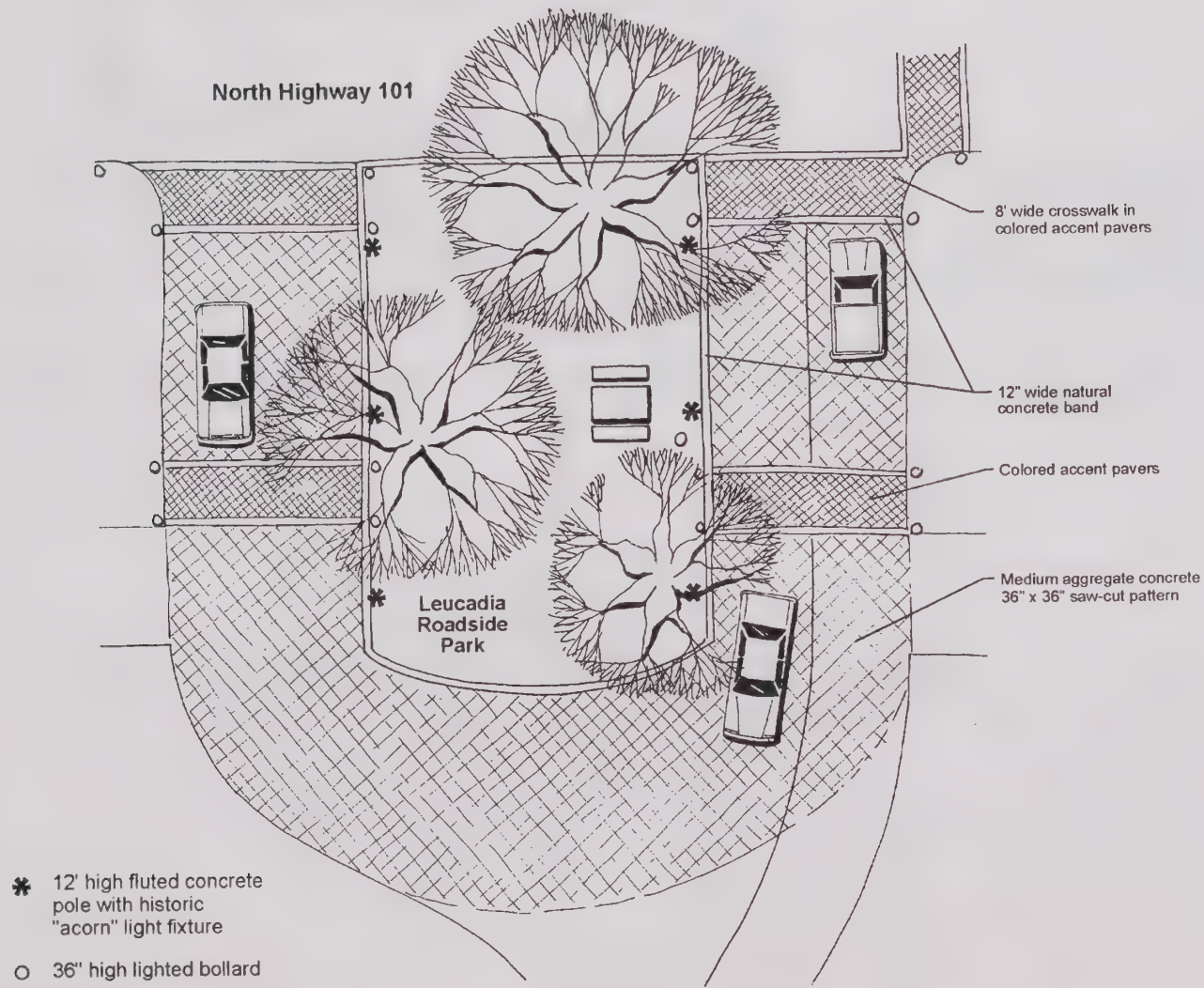


Figure 4-F: Leucadia Boulevard/North Highway 101/North Vulcan Avenue Intersection Concept

Figure 4-G: Leucadia Roadside Park Streetscape Concept



North 101 Corridor Specific Plan


Comprehensive Streetscape Concept Plan

Legend

-  Leucadia Roadside Park
-  Decorative Accent Paving at Crosswalks
-  Entry Sign Location
-  Highway 101 Streetscape Concept
-  Linear Park/Coastal Rail Trail
-  Specific Plan Boundary

Figure 4-H

300 0 300 600 Feet




This map was produced by the City of Encinitas GIS Division and is designed for internal use only. The map is based on the best data available at the time of production and is not guaranteed to survey accuracy. City of Encinitas, RUIS, SANDAG and other data may be represented. Proprietary Information: Access to and use of this information is restricted by a sublicense agreement. No sale, transfer license, or assignment of this information is permitted.



Figure 4-H: Comprehensive Streetscape Concept Plan

4.8 GENERAL LANDSCAPE RECOMMENDATIONS

Landscaping for commercial and mixed use areas in the North 101 Corridor Specific Plan area is important in defining specific areas by helping to focus on building entrances, parking lots, defining the edges of various land uses, providing transition between neighboring properties (buffering), and providing screening for loading and storage areas. Landscaping can be used as a unifying element within a project to obtain a cohesive appearance and to help achieve compatibility of a new project with its surroundings.



4.8.1 GENERAL LANDSCAPE DESIGN RECOMMENDATIONS

- A. Landscaping can be in the form of planting beds, raised planters, containers, or window boxes, if a suitable setback is included.
- B. Landscaped areas should generally incorporate plantings utilizing a three tier system; 1) grasses and ground covers, 2) shrubs, and 3) trees.
- C. The following are common planting design concepts which are encouraged:
 1. Specimen trees used in informal groupings or clusters at major focal points.
 2. Use of flowering vines both on walls and arbors.
 3. Use of planting to create shadow and patterns against walls.
 4. Trees to create canopy and shade, especially in parking areas.
 5. Plantings and walls to screen outdoor areas from wind and views of unsightly areas.
- D. All landscaped areas should emphasize the following water conservation techniques:
 1. Limit turf areas.
 2. Install efficient irrigation systems.

4.0 Design Recommendations

3. Improve soil for better water penetration and improved water holding capacity of the soil.
 4. Use of mulch to cover and cool the soil.
 5. Use of drought tolerant plants. Use of cactus, however, shall be very limited and used only in small areas for accent.
 6. Regular maintenance to include pruning, weeding, proper fertilization, pest control, and water schedule adjustments.
- E. Landscaping should be protected from vehicular and pedestrian encroachment by raised planting surfaces, depressed walks, or the use of curbs. Concrete mow-strips separating turf and shrub areas can also be provided.
- F. Special paving such as brick, stone or tile can be used to identify store entrances and provide a visual link with the streetscape within the North 101 Corridor Specific Plan area.
- G. Plants in containers are especially encouraged for areas adjacent to storefronts along walkways.
- H. Vines and climbing plants integrated upon buildings, trellises, and perimeter walls are not only visually attractive but also help discourage graffiti. A few plants to consider for this purpose are: bougainvillea, grape ivy, and wisteria vines.
- I. Landscaping can be used to screen trash, equipment, storage and loading areas from public view.
- J. The use of inorganic groundcover (gravel or crushed rock) is not recommended. If used in small areas, it should be in combination with live plants and limited to an accent feature.
- K. Appropriate, protective staking is important to protect trees from potential vandalism.
- L. Careful consideration of the spacing of plant materials will insure that they do not interfere with the lighting of the premises or restrict access to emergency apparatus such as fire hydrants or fire alarm boxes. Plant spacing can also insure unobstructed access for vehicles and pedestrians and provide clear vision at street intersections.
- M. The following spacing is recommended for trees and large shrubs:
1. A minimum of 5 feet between center of trees or large shrubs and edge of driveway, water meter or gas meter and sewer laterals.
 2. A minimum of 10 feet between center of trees or large shrubs and point of intersection of the edge of driveways and streets or walkways.
 3. A minimum of 10 feet between center of trees and large shrubs to utility poles.

4. A minimum of 8 feet between center of trees or large shrubs and fire hydrants and fire department sprinkler and standpipe connections.

The following mix is recommended as the minimum size for plant materials:

Trees*	50% 24 inch box, 50% 15 gallon
Shrubs*	100% 5 gallon
Groundcover	100% coverage within 1 year

* Larger sizes are encouraged and may be required for specific projects.

4.8.2 ADDITIONAL LANDSCAPE REQUIREMENTS

- A. All new and rehabilitated landscaping projects that require a permit shall comply with the City's Water Efficient Landscape Ordinance (92-40) except for the following exemptions:
 1. Homeowner-provided landscaping at single family and multi-family projects.
 2. Registered historical sites.
 3. Ecological restoration projects that do not require a permanent irrigation system.
 4. Any project with a landscaped area less than 1,000 square feet.
- B. Informal groupings of street trees along North Highway 101 and Second Street shall be provided as follows:

North Highway 101

1. Parkway - west side:
 Ligustrum lucidum (Glossy Privet)
 Tristania conferta (Brisbane Box)
2. Parkway - east side; Linear Park:
 Cupressus macrocarpa (Monterey Cypress)
 Pinus halepensis (Aleppo Pine)
 Pinus torreyana (Torrey Pine)
 Quercus agrifolia (Coast Live Oak)
 Quercus ilex (Holly Oak)
 Tristania conferta (Brisbane Box)
 Eucalyptus
3. Median:
 Pinus torreyana (Torrey Pine)
 Tristania conferta (Brisbane Box)
 Eucalyptus

4. Accent Trees:

Arbutus unedo (Strawberry Tree)
Erythrina humeana (Natal Coral Tree)

Second Street Parkway

Metrosideros excelsus (New Zealand Christmas Tree)

- C. Parkway and median street trees shall be a minimum of 24 inch box in size.
- D. Linear Park trees shall be a minimum of 15 gallon in size.
- E. Accent trees shall be a minimum of 24 inch box in size.
- F. All trees in paved areas shall be provided with "deep root" barriers and shall be planted in a 5' diameter or square tree well with a minimum interior dimensions of 4 feet. Automatic irrigation and metal grates shall also be provided.
- G. Soils testing for agricultural suitability is required on each site and the resulting laboratory recommendations must be followed.

4.8.3 LANDSCAPE INSTALLATION AND MAINTENANCE

All required landscaping shall be properly installed, irrigated, inspected and permanently maintained prior to use inauguration or the issuance of a Certificate of Occupancy, whichever first occurs. The landscaping and irrigation shall be inspected as stated in the procedures and policy for landscaping and irrigation.

- A. Maintenance of approved landscaping shall consist of regular watering, mowing, pruning, fertilizing, clearing of debris and weeds, the removal and replacement of dead plants, and the repair and replacement of irrigation systems and integrated architectural features.
- B. Prior to the issuance of a Certificate of Occupancy, the landowner shall file a maintenance agreement and easement subject to the approval of the City Attorney. The agreement and easement shall ensure that if the landowner, or subsequent owners, fails to maintain the required/installed site improvements, the City will be able to file an appropriate lien(s) against the property in order to accomplish the required maintenance.

4.8.4 SUGGESTED PLANT PALETTE

All property within the North 101 Corridor Specific Plan area which is between the high tide line and Vulcan Avenue lies essentially within the "first zone of exposure." Landscape materials in this area are subject to strong prevailing winds and salt laden air. Some examples of appropriate landscape materials for the first zone of exposure are:

BOTANICAL NAME	COMMON NAME
<u>Trees</u>	
<i>Arecastrum romanzoffianum</i>	Queen Palm
<i>Arbutus unedo</i>	Strawberry Tree
<i>Brahea armata</i>	Mexican Blue Palm
<i>Brahea edulis</i>	Guadalupe Palm
<i>Butia capitata</i>	Pindo Palm
<i>Chamaerops humilis</i>	Mediterranean Fan Palm
<i>Cupressus macrocarpa</i>	Monterey Cypress
<i>Eriobotrya japonica</i>	Loquat
<i>Koelreuteria paniculata</i>	Goldenrain Tree
<i>Metrosideros excelsa</i>	New Zealand Christmas Tree
<i>Pinus halepensis</i>	Aleppo Pine
<i>Pinus torreyana</i>	Torrey Pine
<i>Pittosporum crassifolium</i>	Karo (std.)
<i>Quercus agrifolia</i>	Coast Live Oak
<i>Quercus ilex</i>	Holly Oak
<i>Washingtonia robusta</i>	Mexican Fan Palm
<i>Tristania conferta</i>	Brisbane Box
<i>Eucalyptus</i>	
<u>Shrubs</u>	
<i>Carissa</i> spp.	Natal Plum
<i>Ceanothus</i> spp.	Wild Lilac
<i>Coprosma</i> spp.	Mirror Plant
<i>Dodonea viscosa</i>	Hopseed Bush
<i>Juniperus</i> spp.	Juniper
<i>Leptospermum laevigatum</i>	Australian Tea Tree
<i>Melaleuca</i> spp.	None
<i>Metrosideros villosus</i>	None
<i>Myoporum laetum</i>	None
<i>Pittosporum crassifolium</i>	Karo
<i>Pittosporum crassifolium nana</i>	Dwarf Karo
<i>Pittosporum tobira</i>	Tobira
<i>Pittosporum tobira</i> Wheeleri	Wheeler's Dwarf Tobira
<i>Raphiolepis</i> spp.	Indian Hawthorne
<i>Rhus integrifolia</i>	Lemonade Bush
<u>Perennials</u>	
<i>Euryops pectinatus</i>	None
<i>Felicia amelloides</i>	Blue Marguerite
<i>Limonium perezii</i>	Sea Lavendar
	Other wild flowers

4.0 Design Recommendations

BOTANICAL NAME	COMMON NAME
<u>Groundcovers</u>	
Arctostaphylos uva-ursi	Bearberry
Baccharis pilularis	Prostrate Coyote Bush
Carissa prostrata	Prostrate Natal Plum
Ceanothus gloriosus	Point Reyes Ceanothus
Ceanothus griseus horizontalis	Carmel Creeper
Delosperma alba	White Trailing Ice Plant
Drosanthemum floribundum	Rosea Ice Plant
Gazania spp.	None
Hedera canariensis	Algerian Ivy
Juniperus conferta	Shore Juniper
Lampranthus productus	Purple Iceplant
Lantana montevidensis	None
Vinca Rosea	Madagascar Periwinkle
<u>Vines</u>	
Bignonia sp.	Trumpet Vine
Ficus Rerens	Creeping Fig
Muehlenbeckia complexa	Wire Vine
Solandra maxima	Cup-of-Gold Vine
Tecomaria capensis	Cape Honeysuckle
Bougainvillea sp.	
<u>Grass and Lily-Like Plants</u>	
Agave spp.	Century Plant
Aloe vera	None
Dracaena spp.	None
Festuca rubra	Red Fescue
Pandanus utilis	Screw Pine
Paspalum spp.	None
Pennisetum clandestinum	Kikuyu Grass
Phormium tenax	New Zealand Flax
Yucca aloifolia	Spanish Bayonet
Zoysia japonica	Korean Grass

5.0 CIRCULATION PLAN

5.1 INTRODUCTION

The Circulation Plan provides directives intended to promote the efficient and safe movement of people and goods within the North 101 Corridor Specific Plan Area. In addition, it establishes policies and programs which will ensure that all components of the transportation system meet the future transportation needs of the City.

- Street improvements for various streets and alleyways located within the specific plan area are proposed. These improvements will be in addition to the improvement concepts outlined in the Streetscape Concepts of the Specific Plan (Chapter 4).
- Transit facilities, provided by the North County Transit District (NCTD), serve the specific plan area.
- Improvements to the bikeway system/network are identified in order to address deficiencies located throughout the specific plan area.
- Pedestrian circulation is addressed within the specific plan area with particular emphasis placed on traffic patterns related to the commercial districts.

5.2 VEHICULAR CIRCULATION SYSTEM

The circulation discussion below reviews existing circulation patterns, the level of service for intersections and proposed street improvements for the streets and alleyways located within the specific plan area.

5.2.1 CIRCULATION STUDY

The Encinitas Traffic Analysis Model (ETAM), originally developed to support the production of the Circulation Element of the City's General Plan, has been refined to focus on the specific plan area. The ETAM has analyzed the traffic circulation patterns and intersection levels of service within the specific plan area relative to the following:

- Previous land uses
- Projected land uses which result from complete buildout conditions associated with current zoning
- Land uses projected as a result of the adoption of the North 101 Corridor Specific Plans and associated street improvements

The circulation model measures the flow of traffic at several strategic intersections located throughout the specific plan area. The unit of measurement used to describe the quality of flow through the intersection is Level of Service (LOS). The LOS is the ratio of an intersection's traffic volumes (existing or forecasted) to its design capacity. For the various intersections, there are six levels of service: A, B, C, D, E, and F. The level of service for intersections is defined by criteria based on the average stop delay per vehicle. Level of Service A represents very little delay (stop delay per vehicle of less than approximately 5 seconds). Level of Service F indicates significant delay (stop-delay per vehicle exceeding 60 seconds).

The analysis of the existing circulation system based upon current land uses (1990) indicates that the Level of Service for various intersections located within the specific plan area is LOS A, B, or C in both the a.m. and p.m. peak hour flow conditions. These values are consistent with the goals for Level of Service criteria outlined in the City of Encinitas General Plan.

The circulation model indicates that two intersections within the North 101 Corridor Specific Plan area are deficient under both the previous zoning buildout conditions and the land uses projected by the specific plan (See Table 5-1). The lane configurations used to determine the levels of service are consistent with the geometrics outlined in the City of Encinitas General Plan.

Table 5-1
Level of Service - Within Specific Plan Boundary

Intersection	Level of Service			
	General Plan Land Uses		Specific Plan Land Uses	
	a.m.	p.m.	a.m.	p.m.
North Highway 101 and La Costa Avenue	C	E	D	E
North Vulcan Avenue and La Costa Avenue	A	D	A	D

Two additional intersections located outside of the specific plan area boundary experience a deficient level of service due to the circulation patterns resulting from the specific plan land uses (see Table 5-2). The lane configurations used are consistent with the geometrics outlined in the City of Encinitas General Plan.

Table 5-2
Level of Service - Outside Specific Plan Boundary

Intersection	Level of Service			
	General Plan Land Uses		Specific Plan Land Uses	
	a.m.	p.m.	a.m.	p.m.
SB I-5 Ramp/Leucadia Boulevard	E	E	D	E
Piraeus/NB I-5 Ramp/Leucadia Boulevard	D	D	D	D

5.2.2 MITIGATION

The Circulation Element of the City of Encinitas General Plan identifies two policies regarding acceptable level of service standards:

Policy 1.2: Endeavor to maintain level of service C as a basic design guideline for the local system of roadways understanding that the guideline may not be attainable in all cases.

Policy 1.3: Prohibit development which results in level of service E or F at any intersection unless no alternatives exist and an overriding public need can be demonstrated.

Through the application of these policy statements, the following recommendations for intersection mitigation are offered.

A. North Highway 101/La Costa Avenue Intersection

The ETAM indicates that the northbound Highway 101 through movement and the southbound Highway 101 left-turns are critical. These traffic movements are conflicting and the resulting impact creates an anticipated level of service which is unacceptable under the land uses projected by the specific plan.

North Highway 101 is identified in the City of Encinitas General Plan as a four-lane major arterial and La Costa Avenue is designated a four-lane collector. The existing intersection lane configurations are inadequate to accommodate the amount of traffic anticipated at buildout under the specific plan. With the construction of the recommended lane configurations shown in Figure 5-A, the level of service for the North Highway 101/La Costa Avenue intersection improves from LOS D (a.m.) and LOS E (p.m.) to LOS A (a.m.) and LOS B (p.m.) at buildout under the specific plan.

B. North Vulcan Avenue/La Costa Avenue Intersection

The ETAM results show that eastbound and westbound Vulcan Avenue through volumes are critical. The level of service predicted is relative to the existing lane configurations, which are inadequate to accommodate the amount of traffic anticipated at buildout under the specific plan. La Costa Avenue is identified by the Circulation Element of the City of Encinitas General Plan as a four-lane collector. With the construction of the recommended lane configurations shown in Figure 5-B, the level of service for the North Vulcan Avenue/La Costa Avenue intersection improves from LOS A (a.m.) and LOS D (p.m.) to LOS A (a.m.) and LOS A (p.m.) at buildout under the specific plan.

C. SB I-5 Ramp/Leucadia Boulevard and Piraeus/NB I-5 Ramp/Leucadia Boulevard Intersections

The design of the improvements to these intersections is currently underway. Improvements include Leucadia Boulevard as a four-lane facility, with turn lanes within the area influenced by the I-5 freeway ramps. The improvements start approximately 300 to 400 feet east of the northbound I-5 ramps and continue westward to approximately 300 to 400 feet west of the southbound I-5 ramps. Construction of the improvements is expected to start in 1998, with completion scheduled for 1999. With the completion of these improvements the southbound I-5 Ramp/Leucadia Boulevard and the Piraeus/NB I-5 Ramp/Leucadia Boulevard intersections will operate at acceptable levels of service.

5.2.3 STREET IMPROVEMENTS

Standards for the improvement of streets and roadways located within the North 101 Corridor Specific Plan are provided below. On-street parking may be restricted, as deemed necessary by the City Engineer, to provide for safe travel on narrow roads, and sight distance and turning movements at intersections and driveways. These standards shall apply only to that portion of the below listed streets located within the boundaries of the North 101 Corridor Specific Plan.

Table 5-3

Streets and Roadways	ROW Width
"A" Street	80' (figure 5-F)
Andrew Avenue	50' (figure 5-H)
Ashbury Street	50' (figure 5-H)
Athena Street	40' (figure 5-J)
Avocado Street	40' (figure 5-J)
Basil Street	45' (figure 5-I)
Cadmus Street	40' (figure 5-J)
Daphne Street	40' (figure 5-J)
Diana Street	40' (figure 5-J)
El Portal Street	50' (figure 5-H)
Europa Street	40' (figure 5-J)
Glaucus Street	50' (figure 5-H)
East Glaucus Street	50' (figure 5-H)
Grandview Street	56' (figure 5-G)
Hillcrest Drive	50' (figure 5-H)
Jason Street	40' (figure 5-J)
East Jason Street	50' (figure 5-H)
Jasper Street	40' (figure 5-J)
Jupiter Street	50' (figure 5-H)
La Costa Avenue	84'
La Veta Avenue	50' (figure 5-H)
Leucadia Boulevard (West)	56' (figure 5-G)
Leucadia Boulevard (East)	92'-101'
Marcheta Street	50' (figure 5-H)
Melrose Avenue	40' (figure 5-J)
North Court	50' (figure 5-H)
North Highway 101	90' (figure 5-C)
Orpheus Avenue	56' (figure 5-G)
Phoebe Street	40' (figure 5-J)
Sanford Street	50' (figure 5-H)
Second Street	80' (figure 5-F)
Sunset Drive	40' (figure 5-J)
Union Street	50' (figure 5-H)
North Vulcan Avenue	
North of Union	55' (figure 5-D)
South of Union	50' (figure 5-E)

A. Alleyways

The existing alleyways located within the North 101 Corridor Specific Plan area are contained within 15-foot and 20-foot right-of-way widths.

The alleyway located north of El Portal Street and between La Mesa Avenue and La Veta Street is contained within a 15-foot right-of-way width. This alleyway should be improved as shown in Figure 5-K. The entire width of the right-of-way should be paved and a concrete valley gutter should be installed along the centerline to facilitate drainage.

The remaining alleyways located within the plan area are contained within a 20-foot right-of-way width. These alleyways should be improved as shown on Figure 5-K. The entire right-of-way width should be paved and a concrete valley gutter should be installed along the centerline to facilitate drainage.

B. Controlled Intersections

Signalized intersections within the plan area are at the following locations:

1. North Highway 101/La Costa Avenue
 - Recommended improvements are described in Section 5.2.2
2. North Highway 101/Leucadia Boulevard
 - No additional improvements are recommended
3. North Vulcan Avenue/Leucadia Boulevard
 - No additional improvements are recommended

No additional signals are warranted at this time.

Three-way stop sign control is currently installed at the intersection of North Highway 101 and Marcheta Street. No other controlled intersections are planned or appear warranted at this time. For pedestrian safety purposes any proposed marked pedestrian crosswalks should be installed only at controlled intersections. Note below that a marked pedestrian crosswalk is planned at Grandview and North Highway 101. The City Engineer may propose, and the Traffic Commission and the City Council may approve new, relocated or modified signalized or controlled intersections in the future.

5.3 TRANSIT FACILITIES

The Circulation Element of the City of Encinitas General Plan contains several policies regarding the utilization of alternative modes of transit. These policies are incorporated into various sections of the North 101 Corridor Specific Plan.

The planned transit facilities within the North 101 Corridor Specific Plan Area are primarily bus transit services. The plan area is currently served by the North County Transit District (NCTD).

Corridor Route 301 provides bus service northward on North Highway 101 to the Oceanside Transit Center and southward on North Highway 101 to University Towne Center. Local Route 361 is strictly an Encinitas route. This bus route traverses throughout the City and services the plan area on North Vulcan Avenue.

5.4 BICYCLE FACILITIES

The recommended bikeway system within the plan area consists of a combination of a Class I bike path and Class II bike lanes. The predominantly used bike route within the plan area is the North 101 Corridor.

Recommendations:

- Provide a multi-modal recreational path within the railroad right-of-way east of North Highway 101 (see Section 4.7 Streetscape Concepts). This facility will be designed as a Class I bicycle path. This bike path will replace the existing inadequately designed Class I facility generally located along North Highway 101 and El Portal Street.
- Provide a Class II bike lane along the northbound and southbound lanes of North Highway 101 (see Section 4.7 Streetscape Concepts). A bike lane will be provided along the southbound travel lanes and parallel parking will be permitted on the west side of North Highway 101. A bike lane will also be provided along the northbound travel lanes and no curbside on-street parking will be permitted along the east side of North Highway 101. Note that the parking alcoves along the east side of Highway 101, which are included in the streetscape concept, will be allowed and shall be designed to accommodate the Class I bike path within the railroad right-of-way and the Class II bike lane along North Highway 101.
- Provide a Class II bike lane along the northbound and southbound travel lanes of North Vulcan Avenue. On-street parking will be permitted on the east side of North Vulcan Avenue (see Section 4.7 Streetscape Concepts).

The recommended bikeway system within the plan area is consistent with facilities outlined in the Circulation Element of the City of Encinitas General Plan, and the Master Bikeway Plan and Engineering Feasibility Study for the City of Encinitas (MBP) prepared by Barton-Ashman Associates, Inc., November 1990.

5.5 PEDESTRIAN FACILITIES

5.5.1 RECOMMENDATIONS:

- Create a safe, convenient circulation network
- Ensure sensitivity to the needs of the handicapped
- Accommodate safe pedestrian facilities adjacent to and crossing the railroad
- Provide adequate spacing between pedestrian and vehicular facilities
- Promote pedestrian access to coastal areas

Along North Highway 101, the existing sidewalk along the western edge is to remain. At those locations where no sidewalk exists, a minimum eight-foot wide decorative sidewalk shall be installed. A multi-modal recreational path, designed as a Class I bicycle path will be provided along the east side of North Highway 101, within the railroad right-of-way. The linear park streetscape concept incorporates this facility, (see Section 4.7 Streetscape Concepts).

5.5.2 MARKED PEDESTRIAN CROSSWALKS ON NORTH HIGHWAY 101

Marked pedestrian crosswalks should be installed and/or continue in use at the following intersections:

- La Costa Avenue
- Grandview Street
- Leucadia Boulevard
- Marcheta Street

Additional marked pedestrian crosswalks may be installed in the future, as recommended by the City Engineer, and as approved by the Traffic Commission and City Council. Crosswalk warning signs should be provided in advance of all marked pedestrian crosswalks.

Parking alcoves as shown in the Streetscape Concepts of the specific plan (see Section 4.7 Streetscape Concepts) may be installed at Marcheta Street, Leucadia Boulevard, Grandview Street, and La Costa Avenue intersections. For pedestrian safety purposes any proposed parking alcoves and their associated pedestrian crossings should generally be installed only at controlled intersections.

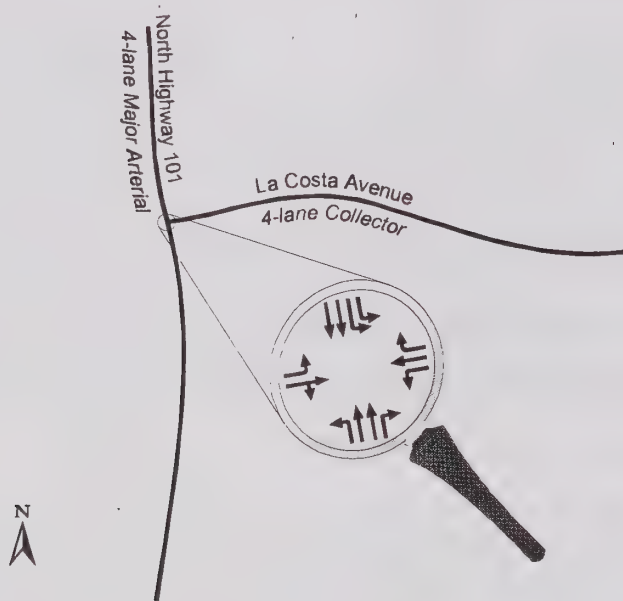


Figure 5-A
North Highway 101 / La Costa Avenue Intersection
Recommended Lane Configuration

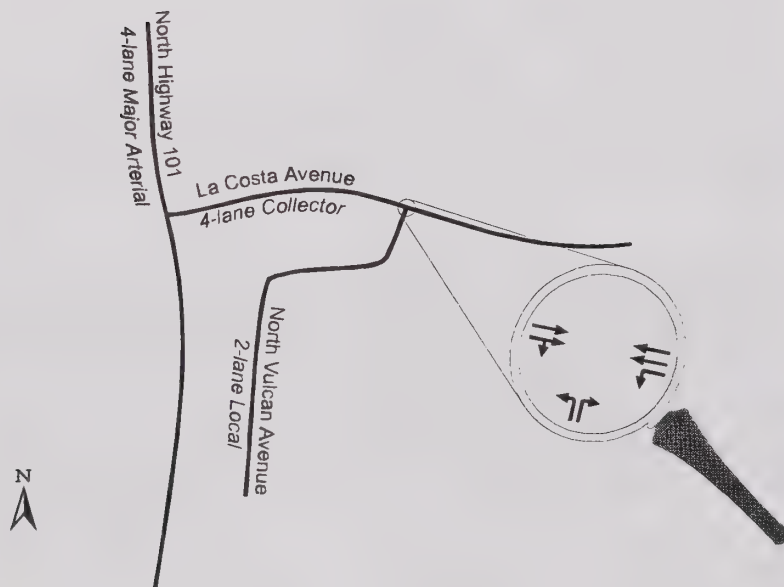


Figure 5-B
North Vulcan Avenue / La Costa Avenue Intersection
Recommended Lane Configuration

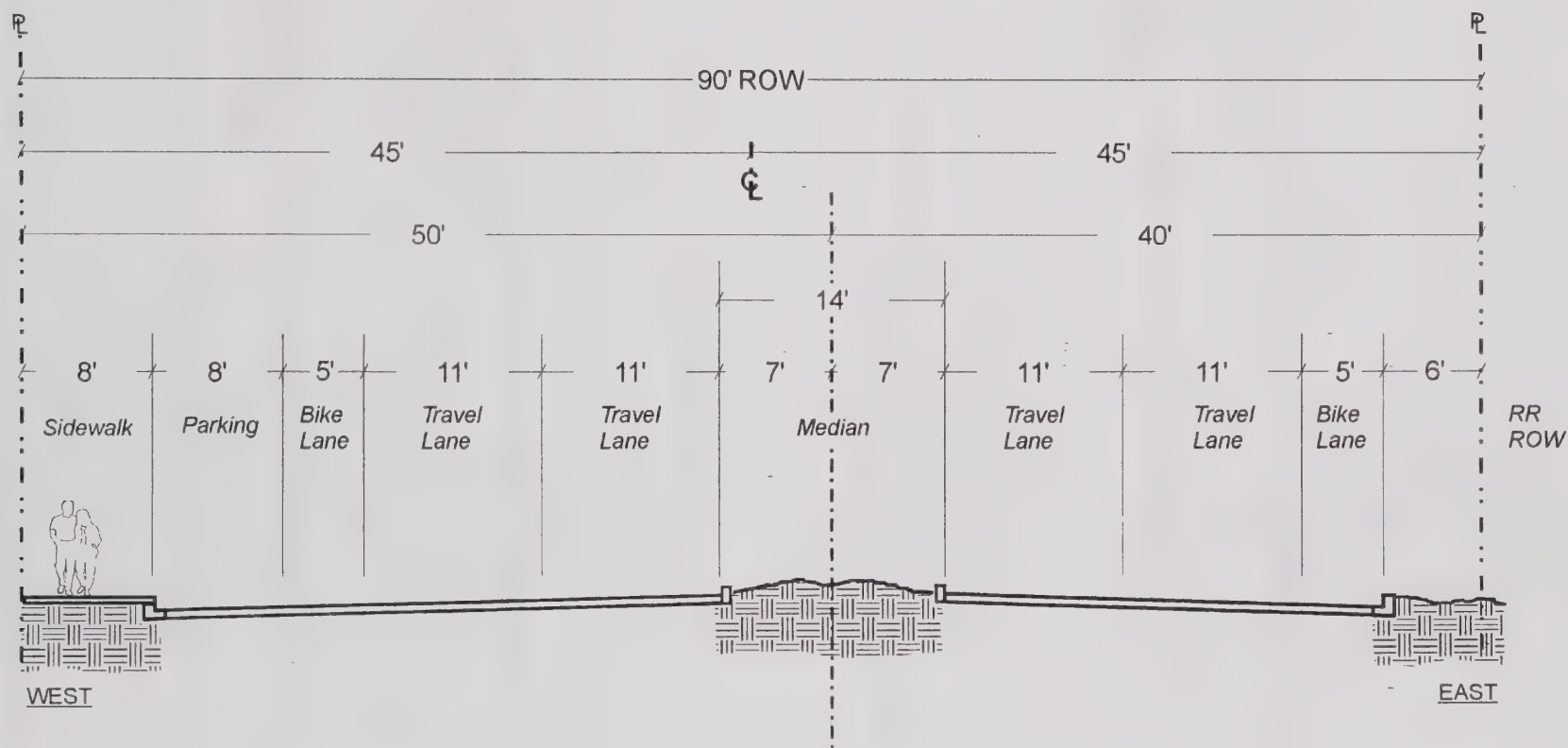


Figure 5-C
North Highway 101 - 90-Foot Proposed Right-of-Way

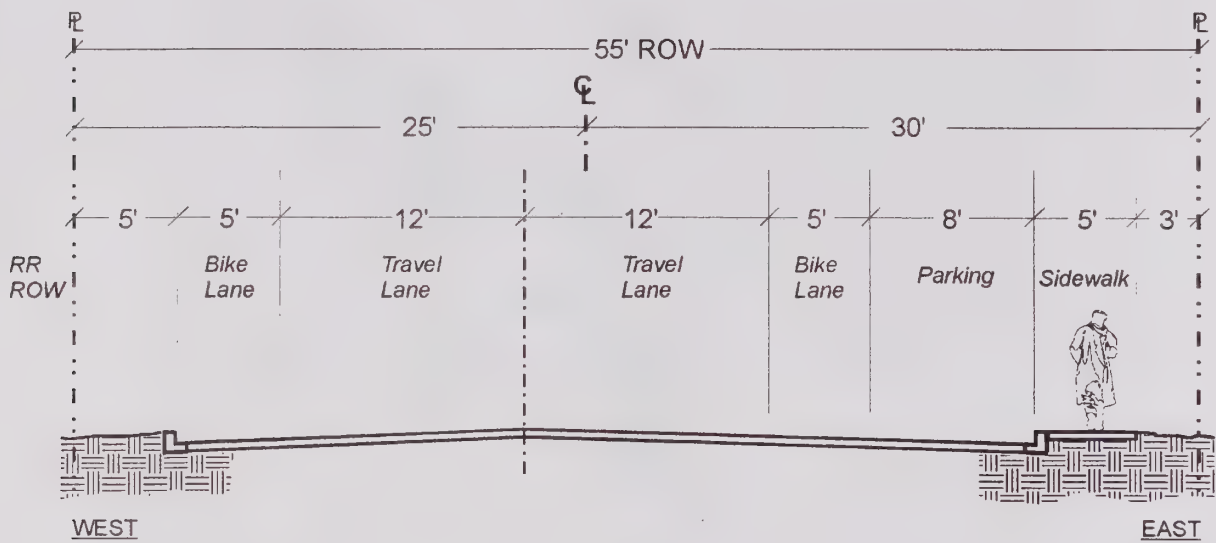


Figure 5-D
North Vulcan Avenue - North of Union Street
55-Foot Proposed Right-of-Way

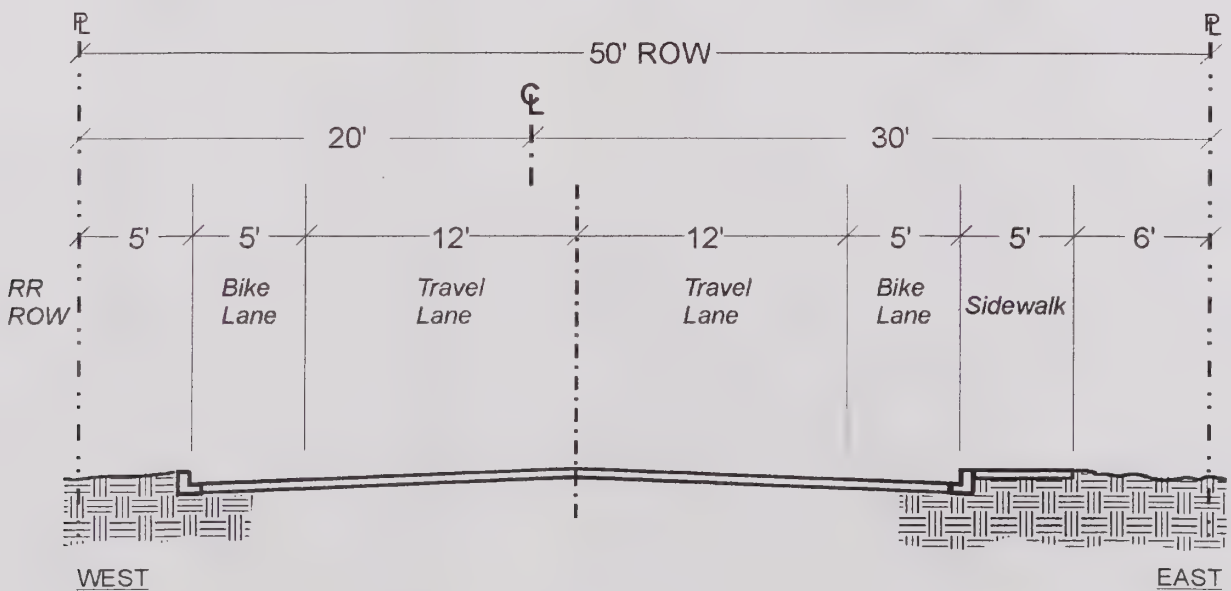


Figure 5-E
North Vulcan Avenue - South of Union Street
50-Foot Proposed Right-of-Way

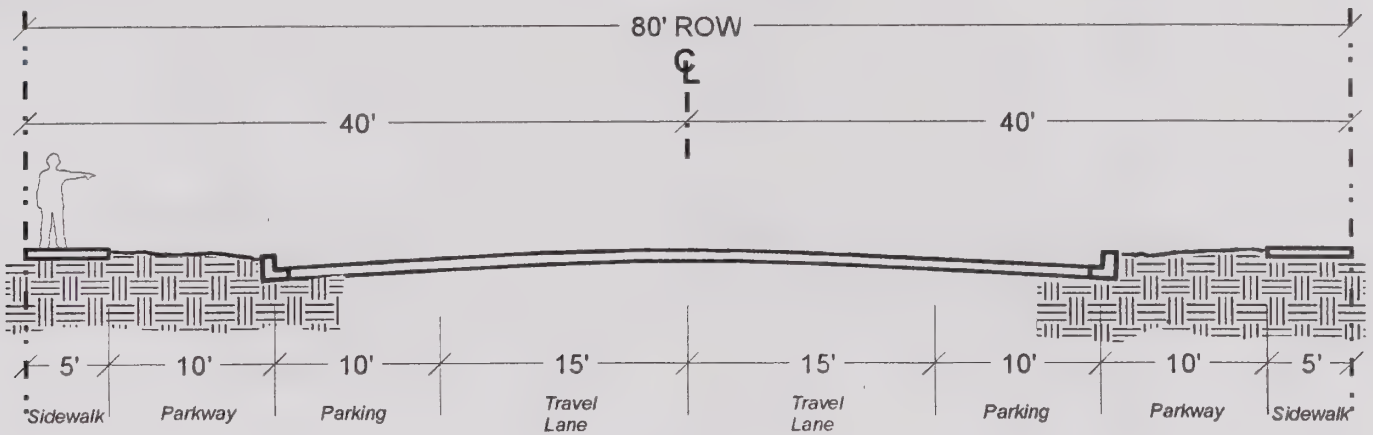


Figure 5-F
"A" Street/Second Street
 80-Foot Proposed Right-of-Way

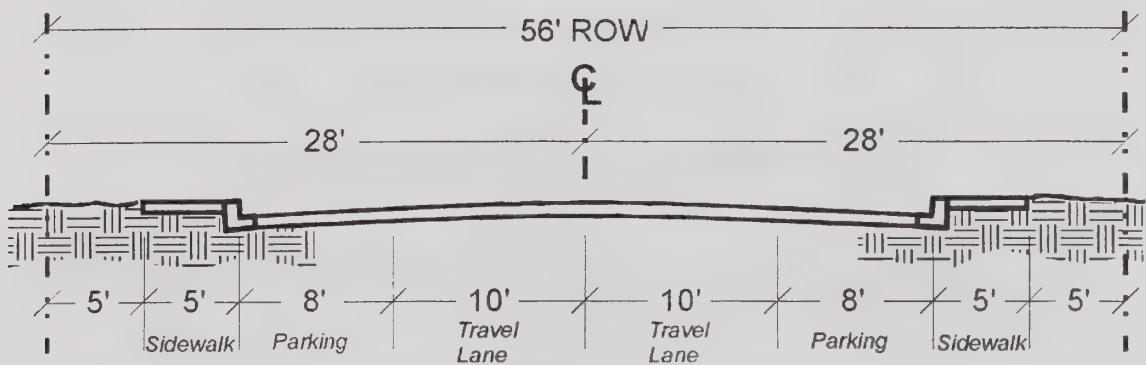


Figure 5-G
Grandview Avenue/Leucadia Boulevard (West)/Orpheus Avenue
 56-Foot Proposed Right-of-Way

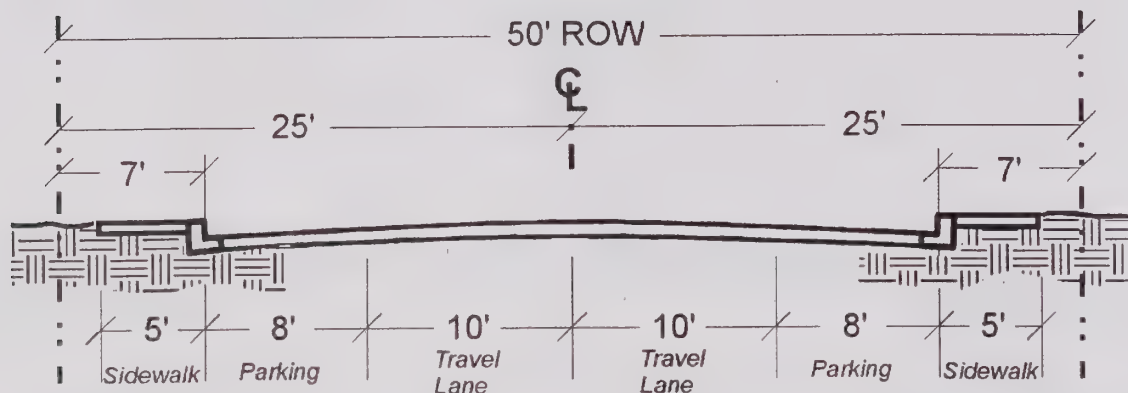


Figure 5-H
**Andrew Avenue/Ashbury Avenue/El Portal Street/East Glaucus Street/
 Glaucus Street/Hillcrest Drive/East Jason Street/Jupiter Street/
 La Veta Avenue/Marcheta Street/North Court/Sanford Street/Union Street**
 50-Foot Proposed Right-of-Way

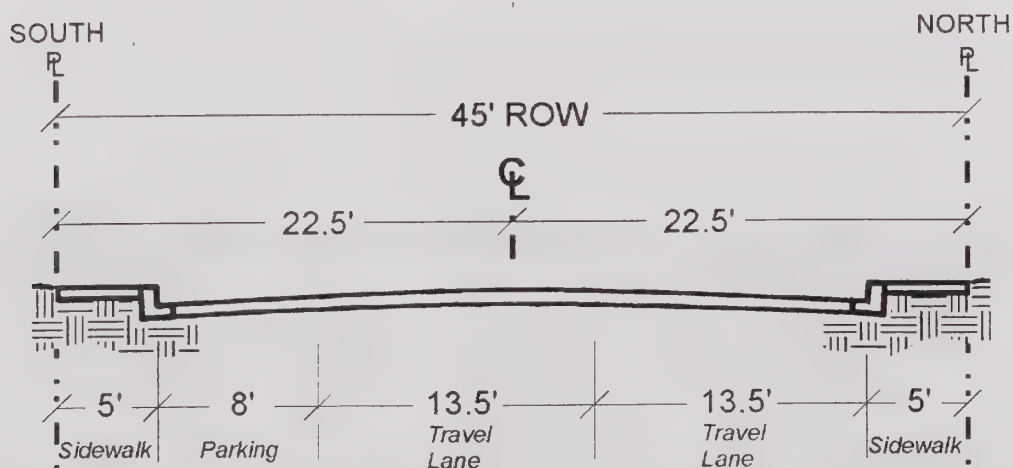


Figure 5-I
Basil Street
 45-Foot Proposed Right-of-Way

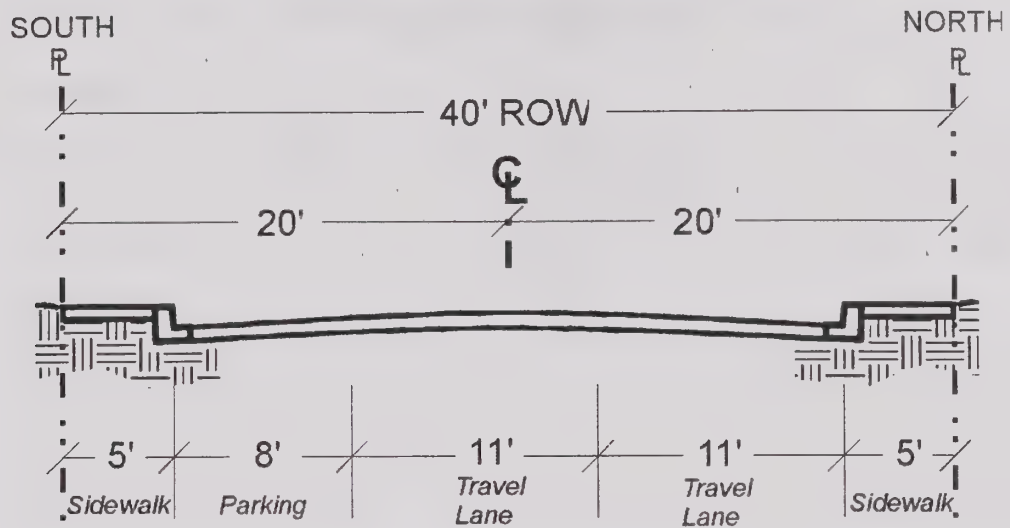


Figure 5-J

**Athena Street/Avocado Street/Cadmus Street/Daphne Street/
Diana Street/Europa Street/Jason Street/Jasper Street/
Melrose Avenue/Phoebe Street/Sunset Drive**
40-Foot Proposed Right-of-Way

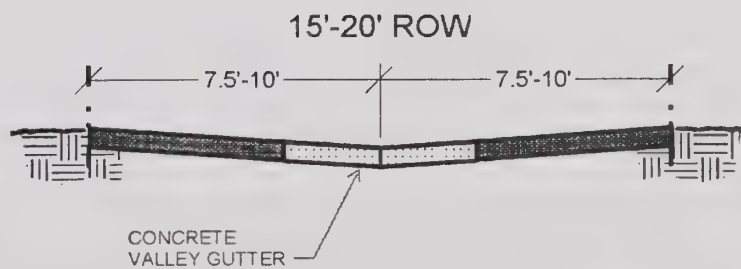


Figure 5-K

Alleyway
15/20-Foot Proposed Right-of Way

Blank Page

6.0 PUBLIC FACILITIES AND INFRASTRUCTURE

6.1 SUMMARY

Public services and facilities include capital facilities (streets, sewers, storm drains and drainage facilities, water, gas and electric utilities) and governmental services (schools, fire and police services, and other governmental services). A goal of the North 101 Corridor Specific Plan is to insure adequate public services and facilities based on the planned types and levels of land use.

Street, alley, pedestrian and streetscape improvements are outlined in the Circulation chapter of the plan. A survey of other existing utility systems was conducted to gauge the need for improvements, based both on age of facilities and demand for additional capacity based on planned area development. The sewer systems were found adequate. Overshadowing other utility needs, however, is the storm drain system. An adequate drainage system needs to be designed and installed to replace the virtually non-existent system today and resolve flooding problems.

So fundamental is the needed storm drain system, that it should be designed and engineered before any other street improvements are engineered or installed. Substantial cost savings may be realized by joint design and installation of drainage, streetscape and other improvements, and a major objective of the specific plan is to define and coordinate all capital improvements. Another infrastructure objective is to underground overhead utilities consisting of electricity, telephone and cable TV lines. The opportunity exists for additional cost savings by coordinating and combining undergrounding with other needed capital facility improvements.

The city, county, and other governmental agencies provide a broad range of public services. Service levels are planned based on projected population and intensities of land use. Police and fire services will not be negatively impacted by the land use changes in the specific plan. This includes the existing system of fire hydrants, which are judged ample in terms of location, capacity and pressure. The elementary and secondary school districts will experience some impact as a result of the proposed specific plan land use. The existing Paul Ecke Central Elementary School on Vulcan Avenue is planned to continue in operation.

Perhaps the greatest challenge for the specific plan regarding public facilities is providing the funding for needed improvements. Substantial capital costs will be involved in needed street, alley, bicycle, pedestrian sidewalk/path, and streetscape improvements, as well as the major storm drain system for the area. Substantial savings can be realized by coordinating all improvements and combining and sharing their funding. The specific plan provides cost estimates for all capital improvements and summarizes potential funding sources to enable subsequent detailed capital facility programming to choose and carry out the best options, as discussed in Chapter 10.

6.2 PUBLIC SERVICES AND FACILITIES

6.2.1 INTRODUCTION

The impacts upon public services created by the North 101 Corridor Specific Plan are minimal. This section of the plan looks at the effect of land use changes to fire service, police service, the county library, school districts, and other public services.

Public services are citywide services provided for the protection, health, and safety of the community's individuals. School and library facilities also are a part of the public facilities. The general plan makes provisions for each of these public services. This specific plan specifically addresses public services and points out any necessary changes to each service as a result of the North 101 Corridor Specific Plan.

6.2.2 FIRE SERVICES

Fire protection to the planning area is provided by the Encinitas Fire Department through Station No. 3 located at 801 Orpheus Avenue. Currently, response time for the plan area is meeting level of service standards. Implementation of the specific plan will not significantly impact the fire services in the area. Existing citywide fire service impact fees should ameliorate any changes to service demand created by changed development intensities in this plan.

6.2.3 POLICE SERVICES

Police protection is provided by the San Diego County Sheriff's Department under contract with Encinitas. The Sheriff's Department Substation, located at 175 North El Camino Real, currently serves the plan area.

Public Safety Element Policy 1.11 of the general plan states the city's goal of one 24-hour deputy per 10,000 residents. Current service levels exceed this standard by providing six 24-hour units per 10,000 residents. Police service will not be impacted significantly as no significant changes to existing development patterns and intensities are taking place.

6.2.4 LIBRARY FACILITIES

Existing library services are provided by the 4,100 square-foot Encinitas Branch of the County Library located at 540 Cornish Drive. Another branch library is located in Cardiff at the Cardiff Towne Center which totals 1,540 square feet. Existing library facilities do not meet county library standards which is calculated as 0.35 gross square feet of library space per person. Based on a 2010 population projection of 65,600, the City of Encinitas will need library facilities totally approximately 22,960 square feet. These additional facilities will be needed to serve the entire city as well as the specific plan area.

6.2.5 SCHOOL FACILITIES

The specific plan area is served by the Encinitas Union School District, San Dieguito Union High School District, and Mira Costa Community College District. The individual schools serving the specific plan area are Paul Ecke Central Elementary School, Oak Crest Junior High School, San Dieguito High School Academy, La Costa Canyon High School and Mira Costa Community College.

The proposed specific plan build-out could potentially add 166 elementary students based on generation rates of .39 students/single-family dwelling unit and .20 students/multi-family dwelling unit. The specific plan could potentially add 77 junior high students and 160 high school students based on generation rates of .12 students/dwelling unit and .25 students/dwelling unit. These projections are based on the net difference between existing conditions and specific plan build-out of residential units.

The school districts currently collect school impact fees assessed on new development to provide financing for future facilities, however, the current fees do not adequately meet the districts' need for financing the facilities generated by new development. A mitigation agreement with developers will be required to address the impact of development on the school districts and to identify the additional financing of school facilities.

6.2.6 PARKS

The specific plan area is served by the 2.7-acre Orpheus Park located at 482 Orpheus Avenue , the 9.3-acre Paul Ecke Sports Park located at 400 Saxony Road , and the proposed 12.9-acre Moonlight Beach Park Master Plan located at Moonlight Beach between B and C Streets in downtown Encinitas. The City recognizes the need for additional parks and is currently evaluating potential park sites.

6.2.7 OTHER SERVICES

It is not anticipated that other public services such as hospitals, utilities, and general city administration will be impacted by the provisions of the North 101 Corridor Specific Plan.

6.3 INFRASTRUCTURE

6.3.1 INTRODUCTION

The infrastructure is the physical, built structure and facilities of the City. It includes improvements to roadways, lighting, sewers, water and utilities. City services are provided by means of the infrastructure. Infrastructure in an area can add to the value of the land and thus increase the economic viability of an area. The ability of a community to provide these services is directly related to the expansion of the community. Limited infrastructure means limited growth.

For purposes of this plan, infrastructure will include, water, drainage, sewer, gas, electric, telephone, and cable. Infrastructure is essential to the North 101 Corridor Specific Plan because it will provide the means for plan implementation to take place. Without adequate infrastructure in place, development will have to provide certain facilities or be postponed.

Two factors determine the adequacy of existing infrastructure. The age of existing infrastructure, compared to its lifespan, delineates the need for infrastructure replacement. Also, the capacity of a facility relevant to projected growth has been analyzed and capacity limits have been determined. This specific plan outlines the need for infrastructure replacement and expansion in the North Highway 101 corridor.

6.3.2 WATER DRAINAGE/STORM DRAINS

The City of Encinitas provides storm water drainage for the planning area. The existing storm and surface water drainage system serving the North 101 corridor and adjacent neighborhoods is a combination of surface street/alley drainage and underground drain pipes.

The general area is plagued with localized low points in many streets and alleys without adequate drainage facilities to alleviate rainfall and runoff. Linear sumps are located along Vulcan Avenue, the railroad tracks and North Highway 101, and impact many businesses in Leucadia. Except for vertical seepage pits that were installed years ago and are now ineffective, virtually no drainage facilities exist in the area to relieve ponding conditions caused by the sump areas.

An Engineering Feasibility and Cost Estimate study was completed for the Highway 101 Corridor Drainage Improvements Project in May 1995. The report evaluated three alternatives for constructing a trunk line storm drain system to relieve the community of Leucadia from flooding conditions caused by ponding water. The alternatives were evaluated based on cost, constructibility, geotechnical conditions and site constraints.

6.3.3 SEWER SYSTEM

The North 101 Corridor Specific Plan area is served by the City of Encinitas and the Leucadia County Water District. All sewage in the North 101 Corridor Specific Plan area flows north to the Encina processing plant. The existing lines provide adequate capacity for any expected growth scenario.

6.3.4 WATER SERVICE

The specific plan area is served by the San Dieguito Water District. The main water lines in the existing distribution system are 12" diameter pipes and feeder lines are 6" or 8" diameter pipes. Most pipes are made of asbestos cement and do not need to be replaced. Under normal conditions water supply in this area is adequate, however during a drought, cutbacks may be required. Water service for the build-out of the area as now proposed in the specific plan is currently sufficient except as noted below.

The water pipe flowing west along La Costa Avenue to Vulcan Avenue, extending south to Andrew Avenue, will be upgraded from a 6" to a 12" diameter pipe. Being a medium priority project, the upgrade will be extended beyond five years, or evaluated sooner, if development occurs in the immediate area.

Storage capacity for the entire water district is insufficient. According to the build-out projections in the Water District's Master Plan, 28 million gallons (mg) of water will need to be stored by 2014. Existing storage is at 5.5 mg, with a 7.5 mg reservoir currently under construction and another 7.5 mg reservoir in the early design stage. No potential sites for increased storage are located in the specific plan area. Storage capacity is a region wide water district issue and is beyond the scope of the specific plan.

6.3.5 GAS SERVICE

Gas service for the specific plan area is provided by San Diego Gas and Electric (SDG&E). According to staff from the Gas System Planning Section of SDG&E, gas pressure in the specific plan area is adequate to serve the area. Some older gas mains exists, but no replacement is anticipated at this time, and none are scheduled for replacement in the near future. No anticipated problems are foreseen with build-out of the specific plan.

6.3.6 ELECTRIC SERVICE

San Diego Gas and Electric (SDG&E) also provides power to the specific plan area. All existing primary electric lines are above ground. Although no problems are foreseen with build-out of the specific plan, the undergrounding of all primary electric lines is proposed and should be coordinated with proposed streetscape improvements.

Rule 20A, replacement of overhead with underground electric facilities, requires the utility company to pay a portion of undergrounding. This includes undergrounding of electric facilities that are in the general public interest, that have adopted an ordinance creating an underground district that meets scenic criteria and that extend for a minimum distance of one block or 600 feet whichever is lesser.

6.3.7 TELEPHONE SERVICE

Pacific Bell provides service for the North 101 corridor by existing aerial lines and there is adequate capacity. Changes and growth are based on historical observation and changes in trends and no age problem has been identified.

Pacific Bell has its lines on San Diego Gas & Electric poles, and if San Diego Gas and Electric is undergrounding lines then the phone company will also underground lines. Although the phone company does not have a 20A program, they are required to match San Diego Gas and Electric's 20A funds for undergrounding.

Blank Page

7.0 HISTORIC PRESERVATION

7.1 INTRODUCTION AND PURPOSE

7.1.1 INTRODUCTION

In communities across the United States, people are becoming aware of the value in preserving their cultural and historic heritage. This interest in our past can be attributed in part as a response to the modern-day, mass-produced housing and look-alike shopping centers and supermarkets. Many buildings from the past exhibit fine craftsmanship and superior materials so often lacking in contemporary development. Historic preservation efforts can also be seen as an extension of the environmental movement: the realization that precious resources - both natural and man-made - are limited and need conserving.

A community's historic buildings are the tangible links with its past and reflect its unique character. Through these links the community's sense of identity is strengthened. Familiar landmarks establish a sense of permanence and anchor the community as a place in time. Historic preservation activities can also foster civic pride and community spirit.

7.1.2 BACKGROUND

The Encinitas General Plan contains policy statements that call for the identification of the City's historic and architectural resources, and proposes that implementation programs be designed to preserve them. Resource Management Element Policy 7.3 states that "the City will pursue development of an historic resources program to assist in the identification of those buildings, structures, and places within the City that have historic significance."

The Land Use Element of the General Plan defines a cultural overlay zone which applies wherever significant historic resources are identified. The citywide zoning code requires projects affecting known historic resources to study potential negative impacts and provide mitigation to the extent feasible.

Through these policies, the City recognizes the importance of protecting the community's link with the past. The North 101 Corridor Specific Plan addresses this by identifying the historic resources in the specific plan area as well as by developing alternatives for preservation of historic resources. The "Architectural and Historic Resources Inventory", conducted in conjunction with the specific plan process, identifies individual resources of varying degrees of significance.

The purpose of this Historic Preservation section is to generate interest in and awareness of the North Highway 101 Corridor area's varied historic resources, and to focus appropriate attention on how to protect them. The specific plan identifies and proposes potential preservation activities and programs for this area's historic resources.

7.2 HISTORIC DEVELOPMENT OF THE NORTH HIGHWAY 101 CORRIDOR

Early Settlers, Mythology and Merle: The U. S. Civil War had been over for a decade when the first Caucasian settler arrived in the area known today as Leucadia. While early accounts of the North Highway- 101 Corridor are sketchy, **Nathan Eaton** is thought to be the first settler. Arriving in 1875, he set up a homestead just south of Batiquitos Lagoon where he grew crops and kept bees.

In the 1880's the Leucadia Land and Town Company was established. One story holds that a band of English Spiritualists settled in the area. Another version maintains that a promoter of questionable reputation, **Tom Fitch**, came from Nevada to sell land through the **Leucadia Land and Town Company**. Regardless of the origin, the name Leucadia, meaning "a sheltered place", remained.

In 1881, the California Southern along with the Atchison, Topeka, and Santa Fe Railroad built a rail yard in National City and began to lay a rail bed along the coast through Encinitas and Leucadia to Oceanside, and ultimately to San Bernardino. Construction of the railroad brought **James Benjamin Elliott** to Leucadia. Several accounts credit Elliott with planting the cypress and eucalyptus trees that define the 101 Corridor of Leucadia. It is thought that Elliott's motivation to plant the trees was to enhance the real estate value of the area and that he used railroad crews to plant the trees. Upon his termination from the railroad, Elliott purchased a farm in Leucadia and joined with Fitch in promoting the Leucadia Land and Town Company.

Proponents of Leucadia commissioned a plan in 1888 by the surveyor, **O. N. Sanford**, who platted Encinitas. The "Map of North Leucadia" featured a grid with 80 foot streets and avenues and 20 foot alleys. Regular lots in the area west of the railroad and adjacent to the railroad on the east were 50 feet by 120 feet while lots farther east were 146 feet by 300 feet and known as "villa lots." The street names include one for Nathan Eaton (Nathan Street) and several from Greek mythology (Hygeia, Marathon, Neptune).

By 1898, the use of street names derived from mythology had taken hold. A survey map of the "Fifth Road District" shows additional streets south of Marathon, all with names from mythology. The southernmost street started with an "A" (Athena) and the streets progressed in alphabetical order (excluding "K") through Lycurgus Street, which was just south of Marathon.

Another early resident was rancher **E. B. Scott** who supposedly planted the first Eucalyptus trees in the area overlooking the lagoon. Scott named the area "Merle" after one of his sons. The name Merle persisted for years. In the San Diego City and County Directory of 1899-1900 and 1901, Merle was listed with N. A. Eaton as postmaster. There was no listing for "Leucadia" in either directory. Even a much later State Highway Survey Map (1926) shows the area between Nathan and Sanford Streets as Merle: so the name persisted well into the twentieth century.

Water, Roads and Subdivisions: The growth and development of Merle and Leucadia, as with most of the West, is directly linked to the availability of water. In spite of the grandiose dreams of the purveyors of nineteenth century Leucadia, the limited water supply kept a ceiling on development. With the construction of the dam at Lake Hodges and the establishment of the San Dieguito Irrigation District, a reliable source of water became available.

In 1925, the South Coast Land Company advertised that their "policy has always been, never to market land until water is actually on the land, ready for use, roads built, maps completed, and all problems connected with marketing such lands thoroughly worked out." With the widening of

Highway 101 in 1913 and the establishment of the water district in 1924, those elements deemed necessary for subdivision were in place and the South Coast Land Company, with others, went to work.

The South Coast Land Company, the biggest land promoter in the area, subdivided a large area east of Highway 101 between Fulvia (now Leucadia Blvd.) and Sanford in early 1924. The company heralded South Coast Park as "the principal subdivision in point of both area and population containing 1050 acres under irrigation.." Starting in 1925 through the end of the decade almost every issue of the Encinitas Coast Dispatch contained a large display ad for South Coast Park as well as a column entitled "South Coast Park". The column loosely tracked the development of the area offering news of individuals who were building there.

The South Coast Park area had moved west of Highway 101 with the subdivision of Unit 2 in 1925, and Units 4 and 5 in 1927. This area stretched from Athena on the south to Grandview on the north. It was during the 1927 subdivision that the streets of Nathan, Pacific, Pine, Monterey, and Myrtle were vacated and replatted.

The popularity of South Coast Park is reflected in a 1929 Encinitas Coast Dispatch article which said that "R. R. Zachary of the South Coast Land Company states that building restrictions are to be more rigidly enforced in the future. He advises all who contemplate building in South Coast Park soon to get in touch...to talk over building restrictions."

Improvements to the general area and service businesses were needed to further promote urbanization. In 1925, the development of a Civic Center (the area surrounding what is now known as Leucadia Roadside Park) for South Coast Park was promoted to attract businesses. In order to achieve the dream of "building a city", it was necessary to provide business services, according to an advertisement in the Encinitas Coast Dispatch in 1925. This ad tells of immediately needing "a first class general store, a good garage and several smaller shops." Plans for the Civic Center included improvements such as light standards. Another ad boasted that "on Wednesday, August 19, our street lights in South Coast Park Civic Center were turned on for the first time to the great delight of the entire community. This makes South Coast Park Civic Center the best lighted district between Santa Ana and La Jolla."

Another major subdivision was **Seaside Gardens** and **Seaside Gardens Annex** established in 1924 by **Esther Cullen**, **Cora A. Taylor**, and **Amelia C. Briggs**. Seaside Gardens was located to the west of Highway 101 and the Annex to the east. Promotions for Seaside Gardens promised that they would bring in "real people, not joy-riders, any one of whom you would like to have as a neighbor. Some will buy in Seaside Gardens, some will want acreage, we have both" boasted an ad in the Encinitas Coast Dispatch.

In 1928, the entire Seaside Gardens tract was purchased by John P. Mills. The Encinitas Coast Dispatch described the area as "an attractive residential district with sidewalks, curbs, gutters, ornamental light standards and trees already planted."

Avocado Acres No. 2 was subdivided by the Southern Title Guaranty Company in 1924, creating Eucalyptus and Wilstone Streets. Avocado Acres was promoted by the Ed Fletcher Company of San Diego. Fletcher was an influential figure in the history of San Diego county and was involved in developing land, roads and dams. In the teens Fletcher worked for the South Coast Land Company and purchased almost all of the coastal land from Oceanside to Del Mar. He helped to build the dam at Lake Hodges that spurred the formation of water companies and districts.

Agriculture, Avocados, and Flowers: The development of a reliable and ready source of water, combined with the mild coastal climate and entrepreneurial growers resulted in the blossoming of Leucadia. A description from the 1929 Encinitas Coast Dispatch tells that "the Briggs Floral Gardens in the northern part of South Coast Park are now a riot of color - the acres of brilliant flowers are visible for many miles North on the Coastal Highway."

The growth of the floral industry is credited to Thomas McLoughlin who moved from Seattle to Leucadia in 1924. McLoughlin, president of the South Coast Horticultural Association, was instrumental in creating the Encinitas Mid-Winter Flower Shows, an annual event that ran from 1925 through 1935. The exhibition attracted growers from all over the state and featured fruits and vegetables as well as flowers. Dignitaries such as the mayor of San Diego and "numbers of celebrities of the movie industry in Hollywood" attended the events.

A green gold coast of avocado trees was envisioned for the Leucadia area in the middle 1920's. The industry was touted as having "phenomenal growth...having been told of its possibilities as a 'green gold' crop". Only a few years earlier they were called "alligator pear trees" and regarded as a mere novelty.

The growing popularity of the fruit was capitalized upon with the subdivision named Avocado Acres. Advertisements for the subdivision offered to plant "your land with the best varieties of avocados from our own nurseries and care for them for a time at reasonable cost." One could also write away for their "new book, 'The Avocado.'"

Avocado groves were also planted in the South Coast Park area along Vulcan Avenue and spreading out to residential streets such as Encinitas (now Sunset) and Hermes. By 1928, avocado groves were considered "one of the high points of interest for those who came to look over this section of South Coast Park." In the early "pre-water" 1920's, plantings were made by seed. With further experimentation and readily available water, avocado plantings and trees became part of the Leucadia landscape.

From Tents to Cabins to Motor Courts to Motels: The Growth of the Tourist Industry: As the transportation improvement of the railroad in the 1880's brought growth to the North Highway 101 Corridor area, the widespread use of the automobile brought growth in the late 1920's. By the late teens American automobile manufacturers had developed assembly line production methods and were able to sell greater numbers of sturdy, inexpensive cars. These private passenger cars offered a freedom from railroads. Railroad lines were defined by the rails and stations. With an automobile a tourist could travel a variety of routes (initially roads used by horses and wagons) and the driver of the automobile could choose where and when to stop and what to see, limited only by the location and condition of the roads.

This freedom "to start, stop, or change direction at will made the automobile more than a means of moving from one place to another. It offered not only an alternative to railroads but also...the magic of the silver screen. The windshield of any car could be transformed into a proscenium arch framing one of the most fascinating movies of all - the landscape played at high speed." (Liebs). No previous mode of transportation provided such an experience.

The combination of the availability of the private automobile and California's climate opened the doors to the development of a tourism industry in the Encinitas-Leucadia area. Highway 101, the state highway connecting Los Angeles and San Diego was the main approach to the area from the North. The eucalyptus and cypress trees that had been planted a few decades earlier provided a shaded roadside strip for travelers who had just traversed the wide open coastal plains. Services for travelers such as gas stations, produce booths, tourist camps, and restaurants began to sprout up along the roadside. From the mid 1920's through the 1930's,

the North Highway 101 corridor spawned a myriad of roadside businesses as the popularity of the automobile endured the depression.

One of the primary services required by travelers were overnight accommodations. One early form of accommodation was the auto camp. Sunset magazine extolled the virtues of "Tenting on the New Camp Ground" (July 1925) describing it as a "pleasure that is being enjoyed by annually increasing thousands in the auto" with new camps being developed "every year for the growing army of motorists who have caught the fever of outdoor life."

In 1925, the South Coast Land Company opened "a modern free camping ground on the Highway" (Encinitas Coast Dispatch, 1925) to accommodate the visitors who were interested in purchasing property. In 1928, the Encinitas Coast Dispatch noted that "although this part of San Diego county is essentially an agricultural unit, the 'vacation industry' and home-making business play an important role in the growth recorded here during the past year. Along the coast highway, hotels, restaurants and other establishments catering to vacation tourists have shown marked expansion."

Some other highway services noted in the Encinitas Coast Dispatch included: a campground and store (Glaucus and Highway 101) managed by Evelyn Hilton and George Calvert; the Journey's End Auto Camp; the Leucadia Service Garage adjoining the Post Office; the Evans Steak and Chop House; the Williams Grocery Store; and the Cypress Grove Auto Camp and Filling Station. During this period another building type critical to the success of automobile touring - the gas station - came into prominence. The one stop service station became a roadside fixture housing a gas distribution system and auto maintenance services under one roof.

By the 1920's tourism had become an integral part of the economy of the North Highway 101 corridor, and the auto campgrounds provided refuge for hardy auto travelers. Traditional hotels in the business districts near rail stations had long offered accommodations. However, after a long day's drive and adventure many motorists preferred the informality of the auto camp to the more formal hotel with its central lobby area. Many communities and businesses such as the South Coast Land Company built tourist camps in city parks or vacant lands. These campgrounds were popular with tourists since they provided reassurance of a comfortable and secure destination where they could gas up the car and browse in local stores. Local businesses also hoped the tourists would stay for a period of time and eat and shop at the local establishments.

However, as auto touring became more popular the camps began to decline. With the influx of tourists it became more difficult to maintain the campgrounds; they became more crowded and occasional unsavory characters camped at these free grounds. Campgrounds began to charge a fee and to provide services and incentives for travelers to stay at their facility. As travelers became conditioned to expect to pay a fee for a place to camp, competition - and the development of amenities - grew. Business owners discovered that travelers were willing to pay additional money for more permanent, private accommodations, and the concept of tent cabins and cabin camps emerged. These, too, were short-lived since the term cabin camps became associated with run down lots full of auto gypsies.

Physical changes from that of haphazard assemblages of tents and cabins to more organized plans of "courts" of cabins resulted in accommodations that were private yet visible and accessible from the highway. The Encinitas Coast Dispatch reported on the early motor courts constructed along Highway 101 in the 1920's. In 1928 it reported that "South Coast Park" is looking forward to the beginning of work on the new McClung auto court which is to be started on the highway near the Civic Center soon. The court is to be of stucco, adding to the plans

drawn up by H. P. McClung of Alhambra...there will be 10 units with garage as well as McClung's own 6-room home." Located north of the Civic Center, "It is one of the most attractive in this section of the community." A year later the Blue Goose Auto Court, "modern in every way", was under construction.

The first motor hotel, called a "motel", is generally attributed to the Milestone Motel built in San Luis Obispo in 1925 by Pasadena architect Arthus S. Heineman. The motel building type consisted of a building (or buildings) each containing a string of rooms rather than the individual cabin concept of motor courts. Motels were more economical to build than motor courts since they maximized available space and cut down on construction requirements. Motels became the primary form of roadside accommodations starting in the 1930's.

Wood Frames and Plastic: The Development of Plant Nurseries: For the first half of the 20th century floriculture in the North Highway 101 Corridor area consisted of growers producing flowers (mainly gladioli) in open fields. The flowers were mostly sold at local markets. There were very few greenhouses in the area. The era of dramatic growth for the floral industry took place after World War II as a result of new technologies. The development of polyethylene plastic film made it possible to construct simple wood frame structures that were enclosed with the plastic film. These wood frame structures were more economical to build than the traditional steel and glass greenhouses. Air freight became a viable option as the cost for the service decreased. This opened up national markets to growers. Growers from the Los Angeles area, squeezed out by urbanization, migrated to San Diego County after World War II.

Conclusion: The early growth and development of the North Highway 101 Corridor is linked to water, transportation, and tourism. After the first footholds of a few early settlers, the coming of the railroad in the 1880's brought transportation links to the rest of California and the United States. Early subdivisions and land development was begun, but was limited by a scarce water supply. The establishment of Lake Hodges and the San Dieguito Irrigation District resulted in a reliable water source. By the mid-1920's several land companies and developers were subdividing the land along the North Highway 101 Corridor. New residents further developed the area by establishing businesses such as agriculture, avocado and flower growing, and developing tourist services such as motor courts, motels and other roadside services.

The cultural landscape of the North Highway 101 Corridor today features surviving examples of these land uses, such as motels and nurseries still featuring their original function in their original form, and structures in which the use has changed but the form remains relatively intact.

7.3 HISTORIC PRESERVATION STRATEGIES

7.3.1 INTRODUCTION

Any successful preservation program must include not only the identification of historic resources, but development of measures to protect them. Potentially landmark-eligible properties and other historic resources have been identified in the "Architectural and Historic Resources Inventory of the North 101 Corridor Specific Plan Area". This section of the Specific Plan outlines basic strategies that can be made available to assist in preserving the community's historic resources.

There are two broad approaches to historic preservation: one is regulatory, and the other is incentive-based. Since most of the resources listed in the inventory are under private ownership, and because there are real economic implications of preservation, a strictly regulatory approach is difficult to achieve. Furthermore, the local community has made clear its desire for a preservation program that is largely incentive-based and voluntary. With this in mind the following strategies for a preservation program were developed.

7.3.2 REGULATORY MECHANISMS

Beyond this specific plan, there are existing City and State regulations in place that provide a level of protection for identified historic resources. These regulations provide both restrictions for preservation, and a relaxing or restructuring of restrictions (such as development and parking standards) to achieve preservation.

A. California Environmental Quality Act (CEQA)

Virtually all development projects are subject to the environmental review process established by the California Environmental Quality Act (CEQA). Projects that would destroy or substantially alter a known historic resource could be deemed to pose a significant effect on the environment. Under CEQA, significant effects must be minimized to the extent feasible. Mitigation measures will vary depending on the project, but can range from photo-documentation or a narrative report recording the resource, to preservation by a variety of means: maintaining the basic structure while accommodating architecturally compatible additions, or granting a "facade easement" for the exterior appearance of the structure. Sometimes as a last resort, relocation of a historic structure is considered.

The City's General Plan establishes a Cultural Resources overlay that applies to historic resources. This overlay is implemented generally by the Municipal Code, which requires projects involving historical sites or structures to perform a site resource survey and impact analysis to determine a site's or structure's significance and the need for impact mitigation (Municipal Code Section 30.34.050 (A)(1)(b)). This Municipal Code requirement continues to apply in the specific plan area. The "Architectural and Historic Resources Inventory" is not to be used to determine when a site definitely has or does not have a significant architectural or historic resource; a site-specific resource survey per the Municipal Code would be required.

B. Specific Plan Provisions Affecting Historic Resources

There are features of the North 101 Corridor Specific Plan that, properly applied, will positively influence the continued viability of the area's historic resources. The design recommendations and development standards are crafted to preserve and maintain the existing desirable character of the North 101 Corridor Specific Plan area. An important part of that character derives from the area's historic resources.

1. Design Recommendations

The design recommendations of this Specific Plan were developed primarily to maintain and promote the existing unique pedestrian scale and village character of the North 101 Corridor Specific Plan area. Emphasis is placed on ensuring that new development is compatible in terms of bulk and scale to existing desirable development without unduly inhibiting creativity and innovative design. Choice of materials, colors, textures, etc., are also discussed.

The design recommendations provide specific discussion of rehabilitation of historic structures. No construction, demolition, or other development shall take place unless the provisions of the Design Recommendations section of this plan affecting historic rehabilitation have been met. Rehabilitation of existing resources should be done in a manner that preserves the original character and integrity of the structure. Remodeling should be done so that non-historic materials or features are removed in favor of restoring original features and intended uses of properties. Removal or alteration of historic material should be avoided, while deteriorated architectural features should be repaired rather than replaced, whenever possible.

2. Development Standards

In addition to the design recommendations, the specific plan guides development through development standards. Citywide zoning and development regulations sometimes run counter to preservation of historic properties. Contemporary regulations generally require larger lot sizes and setbacks than were traditional for urban areas. Because many of the existing lots are small and narrow, previous citywide standards made infill development difficult. Typically, newer development is found on large consolidated lots necessary to comply with setback, parking, and landscaping requirements. The inability to develop or re-develop on infill areas in a manner consistent with older structures, has eroded some of the village character found to be so desirable by the community.

The development standards for the commercial corridor provides for minimal lot sizes, setback and landscaping requirements. Designed to be more reflective of the existing development pattern and to promote a pedestrian-oriented village scale, the development standards provide for a more flexible building envelope that will enhance the viability of the area's historic structures. This will provide more incentive for rehabilitation and preservation, rather than encouraging

demolition of the North 101 Corridor area's older properties. Additionally, the development standards for the commercial corridor permit a wide mix of both commercial and residential of uses. This broader range of uses will further enhance the viability of existing historic properties.

3. Parking Standards

Generally, the citywide off-street parking requirements make development of the small lots in the North 101 Corridor area difficult. The parking standards provided in Section 3.2 of the Specific Plan are designed to encourage the vitality of the commercial district through the application of a variety of strategies including credit for on-street parking, revised parking ratios, and flexible requirements for building additions and remodels. The streetscape design concepts (Section 4.7) include provisions for improved parking along side streets west of Highway 101, and for small parking nodes along the railroad right-of-way.

By restructuring off-street parking requirements, it will be easier for new businesses to occupy existing historic structures.

C. State Historic Building Code

Certain projects affecting historic structures may utilize the State Historic Building Code. Adopted by the City of Encinitas, provisions of the code allow flexible building standards for eligible structures. Eligible structures include those listed on a national, state, or local register; or listed in an official inventory of historical or architecturally significant resources. This has two advantages: utilization of the code allows rehabilitation that respects and conforms to older building methods, thus preserving the original architectural integrity; it also allows for continued use or adaptive re-use of old buildings that otherwise would not be allowed because of strict code requirements.

Projects eligible under this strategy include those properties listed in the Historic Resources Inventory, and shall be implemented on a case-by-case basis. No other implementation action by the City is required.

7.3.3 INCENTIVE STRATEGIES

In addition to regulatory measures, another basic approach to preservation is based on financial and technical incentives. There are a variety of programs, grants, low-interest loans, tax credits, and deductions available to owners of historic properties. The City shall investigate the feasibility of all available programs, and develop other viable incentive programs for purposes of maintaining and rehabilitating historic properties. This section outlines existing incentives that are available through various state and federal agencies. Many of the incentives take the form of tax credits or deductions.

Some of the existing programs outlined below require that properties be listed on the National Register of Historic Places, or be included on a state or local listing to qualify for incentives. It is not required that the City approve or initiate an application for National Register recognition. The process for placing an eligible property on the National Register may be initiated by any individual or organizational entity. However,

the City will support the efforts of such groups or individuals in having historic resources placed on the National Register.

The City should investigate the feasibility of a local historic registry program, in coordination with local historic preservation organizations. Such a program will be voluntary: any property that is placed on a local register must be done with the owner's consent.

Table 7-1 summarizes the preservation strategies outlined in this plan, and how they may be applied to the various significance categories of the historic resources.

Table 7-1

Historic Preservation Strategies Matrix

Applicable Program/Activity	National Landmark Eligible (1-4)	Local Landmark Eligible (5)	Structures of Merit (6a2)	Of Contextual Value (6a3)
CEQA Review	√	√	√	
Design Recommendations	√	√	√	√
Development Standards	√	√	√	√
Parking Standards	√	√	√	√
State Historic Building Code	√	√	√	
Federal Tax Credits	√ ¹	√ ²	√ ²	√ ²
Federal Tax Deduction for Charitable Easement	√ ¹			
Mills Act Property Tax Adjustment	√ ³	√ ³		
CDBG Eligible Funding	√	√	√	
Facade Grant Program	√	√	√	√
Fee Waivers	√	√	√	

¹ Must be Listed on the National Register of Historic Places.

² A 10% tax credit is available to commercial structures built before 1936.

³ Must be listed on a national, state, or local historic register.

A. Tax Credits

Perhaps the most common preservation incentive takes the form of income tax credits. The federal Tax Reform Act of 1986 provides a tax credit equal to 20% of rehabilitation costs for commercial structures and rental residential buildings. To be eligible, the structure must be listed on the National Register of Historic Places or located within a certified historic district. For structures not on the National Register but which were placed in service before 1936, a 10% tax credit is available. Rehabilitation work must conform to the guidelines established by the Secretary of the Interior for historic structures. Certification requests of rehabilitation work are made through the State Office of Historic Preservation; certifications are issued by the National Park Service.

The credit (either 20% or 10%) is available to the taxpayer for five years from date of completion of rehabilitation work. Rehabilitated property may be depreciated over 27.5 years.

An investment tax credit is also available for substantial rehabilitation of low income housing properties. To be eligible for the credit, rehabilitation must meet certain tests relating to cost per unit, number of units occupied by households with income below area median income, and a 15-year compliance period. The credit amounts to 9% of rehabilitation costs for ten years.

For structures listed on the National Register, no action by the City is required for implementation of this strategy. However, the City and community may promote the availability of this program in coordination with local historic preservation groups.

B. Tax Deductions

Certified historic structures that grant a facade easement as a charitable contribution may deduct the calculated loss in property value from their federal income tax liability. Owners of qualified structures listed on the National Register may deed building facades to nonprofit or government entities in exchange for the deduction. The property owner must maintain the facade and preserve its historic value.

C. Mills Act

The Mills Act Tax Adjustment (California Government Code 50280 et seq.) allows cities to enter into contracts with owners of qualified historic properties to provide for their restoration and maintenance in exchange for a property tax reduction. In order to qualify, as with other incentive programs, the property must be a registered National or local landmark and rehabilitation work must conform to the standards established by the State Office of Historic Preservation. Periodic inspections of the property by the County Assessor, Department of Parks and Recreation, and the State Board of Equalization may be necessary to ensure continued compliance with the contract.

Mills Act contracts generally are effective for a period of 10 years, with a provision for automatic annual renewals after the initial time period. Depending

on the specific circumstances of a property (commercial vs. residential use, mortgage interest rates, etc.), Mills Act tax reductions can be considerable.

Implementation of this strategy would be done on a case-by-case basis, at the initiation of a property owner. The community may promote the availability of this program in coordination with local preservation groups.

D. Community Development Block Grants (CDBG)

Certain rehabilitation projects can qualify for low-interest loans or grants through the CDBG program. Qualifying projects must meet a "national objective" such as providing affordable housing opportunities or improving targeted areas of "blight", as defined by the Department of Housing and Urban Development (HUD). The City currently applies its CDBG allocation primarily to serving low-income households.

To qualify for historic rehabilitation funds, in addition to meeting a national objective, properties must be either listed or eligible for listing on the National Register of Historic places; be designated as a state or local landmark by appropriate law or ordinance; or be listed in a state or local inventory of historic places. In allocating CDBG funds, the City will consider granting preference to projects affecting historically significant structures that also provide affordable housing opportunities. Furthermore, it is hereby established as City policy that any CDBG project affecting an historic site or structure must not result in any significant negative impacts to the historic resource.

Implementation of this strategy would require that the City consider establishing criteria for evaluating and prioritizing CDBG allocations to projects involving historic rehabilitation.

E. Facade Grant Program

Over the years, the Downtown Encinitas Mainstreet Association (DEMA) has administered a grant program to assist Downtown merchants in rehabilitating their storefronts. The City should consider supporting a similar program for the North 101 Corridor Specific Plan area businesses to encourage historic preservation and rehabilitation. Rehabilitation work shall comply substantially with accepted standards (State Office of Historic Preservation) and should be consistent with the design recommendations of this Specific Plan. Such a grant program may potentially be funded through the City's General Fund or through CDBG allocations.

Implementation of this strategy would require funding support from the City, and administration by a qualified organization such as DEMA.

F. Fee Waivers

In order to encourage historic rehabilitation, the City could consider reductions or waivers of processing fees for building permits or design review. Fees may be paid from specially earmarked funds from the General Fund or other eligible sources. Implementation of this strategy would require that the City review which processing fees and which types of projects may be eligible for waivers and/or reductions.

7.3.4 ADDITIONAL MEASURES**A. Historic Preservation Ordinance and Commission**

In the future, the City may find it desirable or necessary to establish an historic Preservation Ordinance and/or Commission in order to further coordinate and implement city-wide preservation strategies. An Historic Preservation Commission (HPC) can have several advantages. It can seek various funding sources and develop additional preservation programs, as well as implement those programs outlined in this Plan. With members meeting certain qualifications, it can enable Encinitas to become part of the Certified Local Government (CLG) program. CLG status will enable the City to qualify for certain other grant funds from the federal government earmarked specifically for the CLG program. These funds can be used to finance a number of preservation programs. Participation in the Certified Local Government program gives preservationists access to technical support from the Office of Historic Preservation and other sources.

A Commission would also have the expertise to review projects that affect historic properties. It could review historic resources to determine their historic significance; review National Register applications; make recommendations to the State Office of Historic Preservation; and play a key role in local landmark designation, should the City wish to consider such designation.

Establishment of an Historic Preservation Commission is not required by this specific plan, but would be consistent with the policies and programs of this plan.

B. Education Programs

Any historic preservation program may include activities to make the community aware of, and appreciate its historic resources. This function is being performed, in part, by private groups such as the Encinitas Historical Society, the San Dieguito Heritage Museum, and Cottonwood Creek Conservancy. To further awareness of our historic resources, the City supports the efforts of such groups. As mentioned previously, such local organizations are instrumental in promoting and carrying out certain preservation strategies.

Blank Page

8.0 HOUSING

8.1 INTRODUCTION

The Land Use and Housing Elements of the Encinitas General Plan direct that the North 101 Corridor Specific Plan consider mixed use zoning as a land use option in the specific plan's commercial districts. This means that residential development can be allowed in districts which previously have been zoned exclusively for commercial uses. The reasons for considering this are many, not related just to housing strategies. Housing has not been a major issue in the crafting of the specific plan, but a serendipitous opportunity exists to aid the City's housing needs through the land use strategies of the North 101 Corridor Specific Plan.

The North 101 Corridor Specific Plan area has historically contributed more than its fair share of affordable housing in the city and the City should not rely on this specific plan to solve the citywide affordable housing problem. However, the strategies outlined below address housing need, within the context of the best land use planning for the area, and the desire to economically upgrade the coastal community.

8.2 GOALS

Chapter 2 of this plan presents the community vision and sets forth the goals of the North 101 Corridor Specific Plan. The goals relating to housing are:

- Provide for flexible land use opportunities such as mixed use.
- Identify land use opportunities for under-utilized land.
- Provide affordable housing opportunities, and;
- Establish development guidelines that encourage high quality housing.

8.3 GENERAL PLAN COMPLIANCE

Chapter 9.0 addresses general plan compliance in detail. In terms of housing, to be consistent with and serve to implement the General Plan, this specific plan must retain at least the same amount and levels of density of residential development potential as established under the Land Use Element. In addition, as provided under the Land Use Element and housing program VI-e of the Housing Element, the specific plan process has addressed mixed use residential under commercial zoning.

8.4 EXISTING CONDITIONS

The North 101 Corridor Specific Plan area covers a long, narrow swath of land surrounding the parallel coastal runs of North Highway 101, the railroad tracks, and Vulcan Avenue. This area incorporates the heart of the original coastal Leucadia community, its commercial activity centers, and some of the immediately adjacent residential neighborhoods. At the time of the land use survey for this specific plan (1990) the planning area included 1,256 total dwelling units, 231 under commercial zoning, 74 under office professional zoning, and approximately 46 of these attached directly to non-residential (commercial or office) uses. This shows that a pattern of mixed commercial/ residential use was established in the area prior to the adoption of this plan.

Fifty-five (4.4%) of the existing dwellings were classified as stand-alone, single-family residential. Forty-six (3.7%) were duplex, 516 (41%) were other multi-family, and 602 (48%) were spaces in mobilehome or trailer parks. This pattern clearly differentiates the residential character of the North 101 Corridor Specific Plan area from that of the "average" city residential situation, which is predominantly single-family detached. The existing North 101 Corridor is an area of broad housing variety, reflecting distinct neighborhoods as part of its make-up. Housing here has a number of direct and indirect relationships to the travel corridor itself, both physically and historically. The housing is generally higher density than the average of the city, but is still predominantly small-lot, small-scale in its built character. This residential make-up is a key element of the eclectic, small-scale, unique character of the North 101 Corridor Specific Plan area.

8.5 HOUSING STRATEGIES

The housing "strategies" of the North 101 Corridor Specific Plan are largely the result of the land use planning developed in Chapter 3.0. In creating the desirable land use regulations for the various zones of the plan, an additional benefit has been a net increase in housing potential. Much of this should be relatively "affordable", market-based housing.

8.5.1 MIXED USE ZONING

There are five distinct commercial mixed use zoning classifications in the North Highway 101 Corridor Specific Plan area. The N-CM-1, N-CM-2, N-CM-3 and N-CRM-1 zones, along the west side of North Highway 101, replace previous commercial-only zoning and the N-CRM-2 zone on Vulcan Avenue replaces previous office professional zoning. The N-CM-1, N-CM-2 and N-CM-3 zones provide for stand-alone commercial or commercial and residential uses at a maximum density of 25.0 dwelling units per net acre on the same property or in the same structure, with the intent of providing opportunities for housing and live/work or artisan loft arrangements, while also mitigating the impacts between commercial and residential uses. The N-CRM-1 zone provides for a variety of development opportunities including 1) stand-alone commercial, 2) stand-alone residential at a maximum density of 25.0 dwelling units per net acre; and 3) mixed use at a maximum density of 25.0 dwelling units per net acre. The N-CRM-2 zone provides for the same development opportunities as the N-CRM-1 zone except that the maximum density is set at 15.0 dwelling units per net acre.

The purpose of this mixed use allowance is to recognize an appropriate "village residential" component along this activity corridor; to add neighborhood market support for commercial uses along the highway; to allow and encourage the "live-work" situations already characterizing this community; and to absorb excess development potential along the corridor which commercial-only zoning would not be able to fill.

There are no regulations which require mixed use units to be restricted so as to be affordable to lower income households. However, mixed use units are generally expected to be attached units with the size and development characteristics of condominium units. As such, it is expected that these mixed use units can be relatively affordable, market-based units available for home-ownership or rental. Note the parking and other incentives available, below, for guaranteed-affordable units.

8.5.2 RIVERIA MOBILEHOME PARK

The Riviera Mobilehome Park on Vulcan Avenue is zoned N-MHP. The N-MHP zone is intended to provide for mobile home park development and redevelopment, with a maximum density of 15 dwelling units per net acre for new or redeveloped parks greater than 5 acres. The zone is recognized as a unique existing land use, with unique housing issues; this mobilehome park is basically accepted as part of the eclectic coastal community.

The following two strategies also apply to the N-MHP zone.

A. State Requirements

1. Prior to the consideration to approve any project, including a tentative subdivision or parcel map, or to issue any permit to convert all or any portion of any park to another use; and prior to the closure or cessation of use of all or a portion of any park, the project proponent or property owner shall prepare and file with the City a report on the impact of the conversion, closure or cessation of use on park tenants for whom the park is their primary place of residence. This report shall at minimum cover the points required by and is subject to the requirements for distribution, time limits, requests for hearing, and notification requirements specified by Calif. Government Code Secs. 65863.7, 65863.8, and 66427.4. As a condition of any City approval, the City may require the mitigation of any adverse impacts on park tenants of very low, low, or moderate income, including but not limited to reasonable costs of relocation.
2. Prior to the consideration to approve any project or issue any permit to convert, demolish or replace all or any portion of an existing park, the City shall determine if any park units or spaces which would be converted, demolished or replaced are or have been occupied as their primary place of residence by households of very low, low, or moderate income as defined by the City Housing Element. No such project approval or permit issuance shall be granted unless the proposed project provides for the same number of replacement dwelling units, affordable to the same proportion of very low, low, and moderate income households. Exceptions to this replacement requirement as noted in California Government Code Sec. 65590 (b) shall apply, subject to a finding by the City that replacement is not feasible.
3. No existing park or portion thereof shall be replaced by a nonresidential use, unless the City finds that no residential use at that location is feasible any longer.

B. Incentive Measures

Aside from this specific plan, adopted policy already obligates the City to address and adopt, as feasible, incentives which would have the result of preserving affordable housing opportunities in mobilehome/trailer parks. This stems primarily from the City's Housing Element, programs IV-e, IV-f and V-c. The nature and extent of such incentives, however, are greatly dependent upon available funding, which is undefined at the time of this plan's adoption. Incentives also should be dependent on the real status of mobilehome/trailer park households; that is, whether park tenants actually use the parks as their primary place of residence and what their income status is. The presumption is made that mobilehomes/trailers which are used only as vacation dwellings or which are occupied by upper income households are not important to preserve as affordable housing resources.

8.5.3 OTHER RESIDENTIAL DEVELOPMENT

Additional residential development potential is provided for under this specific plan, according to the various zones outlined in Chapter 3.0. For the most part, these zones carry over residential density and development standards which were in place under previous zoning. In some instances residential densities were increased to more closely reflect the density of existing residential development and to provide for additional housing opportunities along Vulcan Avenue. Development here also may take advantage of the various affordable housing incentives noted below.

8.5.4 PARKING FOR RESIDENTIAL USES

Under this specific plan, parking requirements for stand-alone residential development are the same as those which apply citywide. Units in mixed-use development, however, are subject to a somewhat simplified parking standard, with no more than two offstreet parking spaces required for any dwelling. In addition, as a voluntary incentive, mixed use units which are guaranteed to be affordable to low or very-low income households are allowed a reduced, one-space-per-unit parking requirement.

8.5.5 CITYWIDE HOUSING PROGRAMS

Through the City's Housing Element and Zoning Ordinance a number of programs are established which are basically incentives for residential developers to provide affordable housing. These include:

- consideration of accessory apartments;
- density bonus provisions;
- allowance for development of emergency and transitional shelters;
- federal housing voucher and similar support programs;
- affordable housing rehabilitation; and others.

All of these programs continue to be applicable to land development and use under this specific plan.

8.6 HOUSING PRODUCTION

It is difficult to project the residential buildout potential for the North 101 Corridor Specific Plan area given the prevalent mixed use zoning. However, certain development assumptions were made in order to estimate the potential residential buildout. (Appendix J, Volume IV)

The maximum residential build-out projection under the specific plan is estimated at 1,761 units. This compares to a total of 1,256 units existing in 1990 and a potential of 851 units had previous general plan land use gone to build-out. The reduction in dwelling units under the general plan land use build-out was due to planned commercial land uses replacing existing dwelling units. The mixed-use allowances make up a substantial proportion of these differences: 472 units under the specific plan, none under previous zoning, and 231 units existing in 1990. Additionally, the replacement of OP zoning with various residential and mixed use zones along Vulcan Avenue, together with increases in certain residential zones along this street contribute to an increase in the number of dwelling units.

As noted above, housing development is not mandated in the specific plan to be guaranteed affordable. Market forces are expected to apply such that much of the Plan's residential development potential can be affordable. As such, adoption of the specific plan helps address the obligation of the City to provide "adequate sites for housing opportunities" for income categories of households with identified need.

Blank Page

9.0 GENERAL PLAN AND LOCAL COASTAL PROGRAM COMPLIANCE

The North 101 Corridor Specific Plan was prepared in accordance with applicable goals, policies and provisions of the general plan, which calls for the preparation of a specific plan for the beach-side commercial corridor within the Leucadia community.

The following are applicable goals and policies listed under each element category of the general plan, followed by a discussion which addresses the specific plan's consistency with the general plan.

9.1 LAND USE

GOAL 1: Encinitas will strive to be a unique seaside community providing a balance of housing, commercial, light industrial/office development, recreation, agriculture and open space compatible with the predominant residential character of the community.

POLICY 1.8: Encourage the types of commercial activities which will supply the community with a diverse economic base.

POLICY 1.9: Maintain a proper balance between acreage of commercial land and population served.

POLICY 1.10: Encourage the reasonable regulation of signs to preserve the basic character of the communities and to avoid adverse effect on property values. New pole and roof signage shall be prohibited. Freestanding monument signs shall be permitted, but shall be limited to a maximum height of eight feet. A variance from this height limitation may be granted by the City based on extreme topographic constraints and a demonstration that all other reasonable alternatives have been considered to provide site/business identification. Detailed regulation and limitation shall be specified in the City's zoning regulations. (Coastal Act/30251/Visual Protection)

POLICY 1.11: Encourage the rehabilitation and redevelopment of obsolete or declining commercial development, in ways consistent with community design and development objectives.

POLICY 1.14: The City will maintain and enhance the Highway 101 commercial corridor by providing appropriate community-serving tourist-related and pedestrian-oriented uses. (Coastal Act/30250)

SPECIFIC PLAN COMPLIANCE

The specific plan provides a broader mix of commercial uses which includes commercial mixed use zones allowing residential as a secondary use in conjunction with commercial uses.

The specific plan design recommendations chapter includes a section on general commercial sign recommendations that encourages signs that promote the desired village scale and pedestrian orientation, while acknowledging and servicing the vehicular element which passes by on Vulcan Avenue and on North Highway 101 at speeds of up to 45 mph (see Section 4.5.6).

Commercial and mixed use zones within the specific plan area have been custom-tailored in order to encourage the rehabilitation and redevelopment of existing buildings in the commercial districts. Setbacks, minimum lot sizes, landscape, maximum lot coverage and parking requirements have been modified in order to encourage a more realistic development of the existing properties in the North 101 Corridor Specific Plan area (refer to Chapter 3.0, Use and Development Regulations).

GOAL 3: **To assure successful planning for future facilities and services, and a proper balance of uses within the city, the City of Encinitas will establish and maintain a maximum density and intensity of residential and commercial uses of land within the city which will:**

(a) provide a balance of commercial and residential uses which creates and maintains the quality of life and small-town character of the individual communities; and

(b) protect and enhance the City's natural resources and indigenous wildlife.

POLICY 3.3: The City will designate land use categories/zones for commercial development which provide for the commercial needs of the community with use and intensity regulations consistent with the goals of this Element.

POLICY 3.4: The City will allow those commercial uses which are necessary to satisfy the current and projected needs of the individual communities as indicated on the Land Use Map and under the policies of this plan.

POLICY 3.5: Commercial areas/zones shall be designated to avoid undue concentrations of commercial development which would increase traffic to levels beyond the current and projected capability of the City's services and facilities to deal with the increased traffic.

POLICY 3.7: With the exceptions described in Policy 3.12, once acknowledged as being consistent with the General Plan and Local Coastal Program, the allowable maximum density of any property designated for residential use shall not be increased except by the affirmative vote of a majority of those voting in the election approving the proposed increase.

POLICY 3.8: With the exceptions described in Policy 3.12, once acknowledged as being consistent with the General Plan and Local Coastal Program, property designated/zoned for non-residential uses shall not be redesignated/rezoned to allow residential uses except by the affirmative vote of a majority of those voting in the election approving the proposed change.

POLICY 3.9: With the exceptions described in Policy 3.12, once acknowledged as being consistent with the General Plan and Local Coastal Program, property designated/zoned for residential use shall not be redesignated/rezoned to any non-residential use except by the affirmative vote of a majority of those voting in the election approving the proposed change.

POLICY 3.10: With the exceptions described in Policy 3.12, once acknowledged as being consistent with the General Plan and Local Coastal Program, property designated/zoned for non-residential use shall not be redesignated/rezoned to allow more non-residential uses or a greater intensity of use except by the affirmative vote of four or more City

Council members based upon a finding that the proposed redesignation/rezoning will not substantially reduce revenues to the City and will not substantially increase traffic burdens on roads and highways.

POLICY 3.12: The following may be considered as exceptions to the requirements for voter approval for specified general plan land use map amendments:

1. Minor adjustments in land use boundaries to correctly reflect property or development site boundaries, which adjustments do not substantially change intended area development potential - as approved by Council by unanimous vote.
2. Changes to land use designations to correct (a) map omissions and (b) mapping errors which are clearly demonstrated to be errors contrary to the intent of the General Plan - as approved by a unanimous Council vote.
3. A change from any land use designation to the Ecological Resource/Open Space/Parks designation, when property has been purchased or land development rights have been secured for land for open space or parks purposes - as approved by a unanimous Council vote.
4. Minor land use designation changes when approved by affirmative vote of four or more City Council members. For purposes of this paragraph, "minor" is defined as changes for which certified environmental review per the California Environmental Quality Act has determined that there will be no unmitigable significant negative environmental impacts, and one of the following apply:

(a) Exception for Decrease in Intensity:

The change is to a category of lower land use intensity or density than the existing category that results in a reduction in intensity. For purposes of this determination the following hierarchy of categories, from higher to lower, is established: Light Industrial, General Commercial, Visitor-Serving Commercial, Transportation Corridor, Local Commercial, Public/Semi-Public, Office Professional, Residential 25, Residential 15, Residential 11, Mobilehome Park, Residential 8, Residential 5, Residential 3, Rural Residential 2, Rural Residential 1, Rural Residential, Ecological Resource/Open Space/Parks.

(b) Exception for Residential Density:

The change applies to 5 acres of land area or less, and is a change from a non-residential to a residential category, or from a residential to another higher-density residential category, which would result in the allowance of ten or fewer additional dwelling units (prior to consideration of any density bonus) for the 5 acre site. The change shall also be determined to be compatible with, and generally not exceeding the density of, surrounding planned land use residential densities.

(c) Exception for Change Within Land Use Class:

The change applies to 5 acres of land area or less, and is a change from one land use category to another when both are within the same class of categories. For purposes of this determination the following classes of categories are established:

1. Rural Residential, Rural Residential 1, Rural Residential 2
 2. Residential 3, Residential 5, Residential 8
 3. Residential 11, Residential 15, Residential 25
 4. Office Professional, Local Commercial
 5. Visitor Serving Commercial, General Commercial
5. Changes in land use designations when approved by affirmative vote of four or more City Council members, to allow projects that provide a significant public benefit. A "significant public benefit" shall be determined by the City Council.

SPECIFIC PLAN COMPLIANCE

The specific plan is not proposing to change the existing residential and commercial uses in such a manner that will impact future facilities and services. One of the major goals of the specific plan is to maintain the village atmosphere and pedestrian character of the North 101 Corridor Specific Plan area which will be implemented through the plan's design recommendations (Chapter 4.0) and use and development regulations (Chapter 3.0).

The bluffs to the north of La Costa Avenue and the mature trees along Vulcan Avenue and North Highway 101 are important resources in need of protection and enhancement and specific goals and objectives have been established which address these resources (see Chapter 2.0 and Section 4.8). The bluffs within the plan area are protected by development standards pursuant to Section 30.34.020 of Title 30 of the Encinitas Municipal Code.

The Specific Plan is proposing to amend the general plan land use designation on fifteen distinct areas within the North Highway 101 Corridor Specific Plan area. Eight of the fifteen proposed general plan land use changes are consistent with Policies 3.7 through 3.11, pursuant to the exception provisions of Policy 3.12.4 as follows: a) the change will result in a decrease in the land use intensity; b) the change applies to 5 acres of land area or less, and is a change from a non-residential to a residential category, or from a residential to another higher-density residential category, which would result in fewer than 10 dwelling units; or c) the change applies to 5 acres of land area or less, and is a change from one land use category to another when both are within the same class of categories.

Seven of the fifteen proposed land use changes are consistent with Policies 3.7 through 3.12, pursuant to the exception provision of Policy 3.12.5, as the changes have been determined to provide a "significant public benefit" and have been approved by affirmative vote of four or more City Council members. The "significant public benefits" provided by these land use changes include the following: 1) the allowance of mixed use and higher density residential development which will provide more opportunities for a variety of housing types; 2) design recommendations directed at preserving and enhancing the village atmosphere and pedestrian character of the North 101 Corridor Specific Plan area, 3) specifically tailored development standards to allow flexibility in the development of new projects and the rehabilitation of existing development; 4) the allowance of mixed use as an incentive for commercial and economic development, and 5) increasing the land area designated for Visitor-Serving Commercial would increase the opportunity for economic benefit to the City. At buildout, the overall land use intensity of the proposed land use plan, as measured from a traffic generation standpoint, is less than at buildout under the current general plan land use plan. The proposed commercial, mixed use and residential zone changes will not pose significant traffic impacts as discussed in Chapter 5.0, Circulation Plan and the Environmental Impact Report (EIR) and Addendum certified by the Encinitas City Council.

GOAL 4: The City of Encinitas will ensure that the rate of residential growth:

- (a) does not create a demand which exceeds the capability of available services and facilities;
- (b) does not destroy the quality of life and small town character of the individual communities;
- (c) does not exceed a rate which excludes the public from meaningful participation in all aspects of land use decision making regarding proposed projects;
- (d) provides the City with the ability to plan ahead for the location, timing and financing of required services and facilities; and
- (e) does not exceed an annual allotment of dwelling units based on the projected ultimate buildout of dwellings in the City of Encinitas assuming a 25-year buildout period.

POLICY 4.2: The City will plan to provide services and facilities concurrent with projected need, sufficient to allow issuance of the maximum annual number of dwelling unit building permits. To guard against an unforeseen shortfall of services or facilities, the City will determine adequacy of services and facilities for each residential development at the discretionary review stage. For dwellings not subject to discretionary review, such determination shall be made with the earliest development permit submittal. No unit shall be approved or permitted for which inadequate services or facilities are available.

SPECIFIC PLAN COMPLIANCE

The specific plan land use build-out proposals will increase the number of dwelling units by approximately 871 dwelling units over the projected general plan build-out for the planning area. However, this increase will not significantly impact public services or facilities with the exception of school and library facilities (see Chapter 6.0).

GOAL 5: The City of Encinitas will ensure that the rate of commercial growth:

- (a) does not create a demand which exceeds the capability of existing services and facilities;
- (b) does not destroy the quality of life and small town character of the individual communities;
- (c) does not exceed a rate which excludes the public from meaningful participation in all aspects of land use decision making regarding proposed projects;
- (d) provides the City with the ability to plan ahead for the location, timing and financing of required services and facilities; and
- (e) provides for the City's commercial needs during growth and thereafter as part of a fully-developed city.

- POLICY 5.1: Commercial growth within the City should be sufficient to support residential growth and provide adequate services to the citizens of the individual communities.
- POLICY 5.2: Proposed commercial growth should reflect a general balance of uses within the community.
- POLICY 5.3: The City shall develop and implement commercial zoning and development standards which will accomplish the goals and policies of the General Plan, emphasizing quality, community character, and community needs.

Specific Plan Compliance

The specific plan build-out proposals may reduce the total general commercial square footage over the general plan build-out projection for general commercial. This is based on assumptions that a certain portion of each property in the commercial mixed use zones would develop with residential as described in Chapter 3.0. However, these proposals are consistent with Goal 5 and Policies 5.1, 5.2 & 5.3 of the General Plan listed above.

The specific plan commercial and mixed use land use proposals will not pose a significant impact to public services and facilities within the planning area, with the exception of school and library facilities, and are consistent with the general plan land use policies for the Leucadia Community.

- GOAL 6: Every effort shall be made to ensure that the existing desirable character of the communities is maintained.**
- POLICY 6.1: Permit commercial land uses and other types of non-residential development only in those areas where such development presently is concentrated or where indicated by the General Plan as long as such development does not result in land use conflicts with surrounding residential development.
- POLICY 6.5: The design of future development shall consider the constraints and opportunities that are provided by adjacent existing development. (Coastal Act/30251)
- POLICY 6.6: The construction of very large buildings shall be discouraged where such structures are incompatible with surrounding development. The building height of both residential and non-residential structures shall be compatible with surrounding development, given topographic and other considerations, and shall protect public views of regional or statewide significance. (Coastal Act/30251/30252/30253)
- POLICY 6.7: Require commercial development to provide sufficient landscaping to soften the visual impact of commercial buildings and parking areas.

Specific Plan Compliance

Chapters 3.0 and 4.0 include use and development regulations and design recommendations, respectively, that consider the opportunities and constraints and visual and physical impacts to surrounding development within the planning area.

- GOAL 7:** Development in the community should provide an identity for the City while maintaining the unique identity of the individual communities. (Coastal Act/30253)
- POLICY 7.1:** Establish design themes for commercial districts located within individual communities.
- POLICY 7.3:** Although common themes for each of the communities may be established, each community may also promote variety in the design of individual projects.
- POLICY 7.4:** Develop regulations and incentives for residential, commercial, and industrial developers to vary design, setbacks, driveways, rooflines, materials, colors, landscaping, etc. to ensure variation in the design of individual residential units within larger subdivisions and commercial/ industrial projects.
- POLICY 7.5:** A streetscape specific plan(s) for the Highway 101 corridor and El Camino Real corridor shall be prepared (Coastal Act/30251)
- POLICY 7.6:** Private development shall coordinate with street/public improvements, i.e. streetscape, landscape, site design and the like.
- POLICY 7.8:** The 101 corridor from the north City boundary to Encinitas Boulevard is designated a Specific Plan area. Development shall be allowed prior to the completion of the Specific Plan.
- Establish standards and uses that compliment adjacent residential uses, enhance the appearance of Highway 101 streetscape and railroad right-of-way, and create an attractive commercial district that would serve the needs of both local residents and visitors to the Community.
 - Establish standards that will help to revitalize the commercial corridor.
 - Provide for residential uses in commercial zones along the corridor provided the residential uses are accessory and incidental to the principal commercial use.
 - Develop standards for development to vary design, setbacks, materials, colors, landscaping and the like to ensure a variation in the design of individual projects that can be tied together via common parking and internal circulation, and a common streetscape design.
- POLICY 7.10:** Both residential and non-residential development shall be limited to a maximum height of two stories and 30 feet. Limited exceptions for non-residential development may be allowed, but only for designated specific sites as developed and adopted through area specific plans. Exceptions may also be made for Medical Complex development projects at the discretion of the City pursuant to conditional use permit applications as provided by the Zoning Code, to allow building heights up to a maximum height of three stories.

Specific Plan Compliance

The primary goal of the specific plan is to maintain the unique and desirable characteristics of the specific plan area, while providing continued private land use and public improvements. The specific plan provides custom-tailored zoning to replace the standard citywide zoning regulations. Use allowances and development standards have been created to fit the particular circumstances of the area which are consistent with the above goal and policies (see Chapter 3.0).

Design recommendations have been established which address streetscape, landscape, commercial and residential development (see Chapter 4.0).

The specific plan has established a building height limit of 33 feet or three stories, whichever is less, to allow additional design flexibility for commercial, mixed use, and residential development projects.

GOAL 9: Preserve the existence of present natural open spaces, slopes, bluffs, lagoon areas, and maintain the sense of spaciousness and semi-rural living within the I-5 view corridor and within other view corridors, scenic highways and vista/view sheds as identified in the resource management element. (Coastal Act/30240/30251)

POLICY 9.3: Prohibit and eliminate billboards and obtrusive advertising media along freeway corridors, Highway 101/First Street and other scenic corridors and routes as specified in the Resource Management Element. (Coastal Act/30251)

SPECIFIC PLAN COMPLIANCE

Section 4.5.6, General Commercial Sign Recommendations, encourage signs that achieve and promote the desired village scale and pedestrian orientation as well as acknowledge and service the vehicular element which passes by on North Highway 101 and Vulcan Avenue at speeds of up to 45 mph. Sign recommendations are provided that are consistent with the above goal and policy.

A streetscape concept and street tree master plan have been established for North Highway 101 (see Sections 4.7 and 4.8 respectively).

9.2 HOUSING

GOAL 1: The City will encourage the provision of a wide range of housing by location, type of unit, and price to meet the existing and future housing needs in the region and city. [i-a through h; ii-a through d; iii-c and d; iv-a through f; v-b through g; vi-a, c through l; vii-c; viii-c]

POLICY 1.2: Strive to provide a wide variety of housing types so that a range of housing needs and tastes will be made available to existing and future residents. [I-a through h; II-b, c, d; IV-e, f; V-b, c; VI-a, e, j, k, l; VII-e; VIII-c]

POLICY 1.3: When existing residential units are replaced, they should be replaced with units that are compatible in design with the surrounding residential neighborhood as planned by the City. [I-a]

- POLICY 1.6: Encourage retention of all existing, viable mobile home parks through use of a mobilehome/trailer park overlay zone, a permanent mobilehome park zone, and the application of incentives. [I-d; II-c; IV-e, f; V-c]

SPECIFIC PLAN COMPLIANCE

The North 101 Corridor Specific Plan implements the City's General Plan and the commitment of the Housing Element to provide a range and balance of housing opportunities. The specific plan area includes the residentially zoned properties along Vulcan Avenue, which will continue to contribute to the City's housing stock. The North 101 Corridor Specific Plan utilizes mixed use zoning which includes residential uses along with commercial uses. (see Chapter 3.0).

Section 4.6 discusses residential design recommendations which encourage good design that is compatible with other well designed buildings without severely restricting the building owner's preference or the dictates of economics regarding materials and style.

Section 8.5.B. summarizes specific plan strategies for the retention of the Riveria Mobile Home Park within the specific plan area.

- GOAL 3: The City will encourage the maintenance and preservation of the existing housing stock as well as quality workmanship in new housing.**

- POLICY 3.9: Encourage street planting, landscaping, and undergrounding of utilities. [I-a]

- POLICY 3.10: Encourage high standards of design, materials, and workmanship in all construction and developments. [I-a]

SPECIFIC PLAN COMPLIANCE

Landscape recommendations and a street tree master plan for North Highway 101 and Second Street have been established for the specific plan area (see Section 4.8).

Section 4.6 discusses residential design recommendations which encourage good design that is compatible with other well designed buildings without severely restricting the building owner's preference or the dictates of economics regarding materials and style.

9.3 CIRCULATION

- GOAL 1: Encinitas should have a transportation system that is safe, convenient and efficient, and sensitive to and compatible with surrounding community character. (Coastal Act/30252)**

- POLICY 1.2: Endeavor to maintain Level of Service C as a basic design guideline for the local system of roadways understanding that the guideline may not be attainable in all cases.

- POLICY 1.3: Prohibit development which results in Level of Service E or F at any intersection unless no alternatives exist and an overriding public need can be demonstrated.

- POLICY 1.4: Require, where feasible, interconnecting offstreet pedestrian and vehicular circulation between adjacent commercial and office land uses. This policy should be required along major transportation corridors to minimize traffic conflicts associated with pedestrian and vehicular movement to and from these properties. (Coastal Act/30252)

- POLICY 1.7: Encourage adjacent properties to use common access points to access prime arterials, major roads, collectors and augmented local streets.
- POLICY 1.9: Minimize private driveway access onto both major and collector roads.
- POLICY 1.10: Encourage the design of roads and traffic controls to optimize safe traffic flow by minimizing turning, curb parking, uncontrolled access, and frequent stops.
- POLICY 1.12: The City will promote increased off-street parking for existing and future commercial and residential uses in the near coast area, will minimize curb cuts for new development in the vicinity of beach access points in order that the maximum amount of curb parking will be available to beach users, and will encourage remote parking/shuttle service and park-and-ride facilities in the Coastal Zone. The City will require that all commercial, industrial and residential uses be designed and constructed with sufficient off-street parking and loading facilities to assure adequate parking is provided with new development such that no adverse impacts on coastal access are documented. Parking ratios shall be utilized as specified and detailed in the City's Zoning Code and in implementing Specific Plans which provide sufficient parking spaces so as not to require patrons/employees/residents to utilize parking which is necessary/required for other approved uses or street and other public parking that should otherwise be available for public use. (Coastal Act/30252)
- POLICY 1.14: A coordinated traffic signal system shall be developed and implemented.
- POLICY 1.15: The City will actively support an integrated transportation program that encourages and provides for mass-transit, bicycle transportation, pedestrians, equestrians, and car-pooling. (Coastal Act/30252)
- POLICY 1.17: Standards shall be established and implemented to provide for adequate levels of street lighting, based on criteria of safety and related to volumes of vehicular, pedestrian and bicycle activity and potential points of conflict. Such standards shall be designed to respect different community and neighborhood needs for lighting, different community standards for design and special attention given to preservation of dark sky.
- POLICY 1.18: Standards shall be established and implemented to provide for a comprehensive system of traffic control devices and signing, based on sound traffic engineering principals, to assure traffic safety and preservation of community character.
- POLICY 1.19: The City will provide for adequate levels of maintenance of all improved components of the circulation system, such as roadways, sidewalks, bicycle facilities, roadway drainage systems, pedestrian, recreational trails, bicycle trails and facilities.

SPECIFIC PLAN COMPLIANCE

The circulation model indicates that two intersections (North Highway 101/La Costa Avenue and Vulcan Avenue/La Costa Avenue) within the North 101 Corridor specific plan area are deficient under both the previous General Plan buildout conditions and the land uses projected by the specific plan. The lane configurations used to determine the levels of service are consistent with the geometrics outlined in the City of Encinitas General Plan. Recommendations for intersection mitigation are offered in Section 5.2.2. With the implementation of the mitigation measures, the level of service for the North Highway 101/La Costa Avenue intersection will improve from LOS D (a.m.) and LOS E (p.m.) to LOS A (a.m.) and LOS B (p.m.) at buildout under the Specific Plan. The level of service for the Vulcan Avenue/La Costa Avenue

intersection improves from LOS A (a.m.) and LOS D (p.m.) to LOS A (a.m.) and LOS A (p.m.) at buildout under the Specific Plan.

Two additional intersections (Southbound I-5 Ramp/Leucadia Boulevard and Piraeus/Northbound I-5 Ramps/Leucadia Boulevard) located outside of the Plan Area boundary experience a deficient level of service. Recommendations for intersection mitigation are offered in Section 5.2.2. With the implementation of the intersection mitigation measures the southbound I-5 Ramp/Leucadia Boulevard and the Piraeus/NB I-5 Ramp/Leucadia Boulevard intersections will operate at acceptable levels of service.

Chapter 4.0, Design Recommendations, provides recommendations that encourage more efficient and compatible circulation patterns within the specific plan area. Alley access for delivery trucks and vehicular access also is encouraged.

Parking alcoves along the railroad right-of-way are proposed in order to provide an increase in off-street parking for commercial businesses along North Highway 101 (see Sections 3.3 and 4.7). Streetscape and circulation proposals include street improvements that will promote the efficient and safe movement of people and goods within the specific plan area (see Section 4.7 and Chapter 5.0).

- GOAL 2: The City will make every effort to develop a varied transportation system that is capable of serving both the existing population and future residents while preserving community values and character. (Coastal Act/30252/30253)**
- POLICY 2.1:** Adopt and implement standards for rural, semi-rural, and urban roadways within the City consistent with community character.
- POLICY 2.4:** When considering circulation patterns and standards, primary consideration will be given to the preservation of character and safety of existing residential neighborhoods. Where conflicts arise between convenience of motorists and neighborhood safety/community character preservation, the latter will have first priority.
- POLICY 2.8:** Where necessary, require acquisition of right-of-way as a condition of approval of all final subdivision maps. Encourage landscaping of rights-of-way if not being used for public roads, hiking/riding trails or beach access trails.
- POLICY 2.9:** Develop new alternate road and intersection standards to promote retention of existing neighborhood atmosphere.
- POLICY 2.10:** Establish landscaping buffer and building setback requirements along all roads which are local augmented status or larger, except where inappropriate. (Coastal Act 30252)
- POLICY 2.12:** Encourage unique characteristic community design standards for traffic signals and intersection signing and other street improvements, structures and furniture.
- POLICY 2.13:** Encourage landscaped medians and parkways on all roadways where practical.
- POLICY 2.14:** Develop rural, semi-rural, and urban standards and criteria for private streets including a mechanism for on-going maintenance.
- POLICY 2.16:** Develop a program to improve alleyways with paving and drainage improvements.

POLICY 2.18: Where possible, functioning of the street system should be improved by the installation of intersection improvements.

SPECIFIC PLAN COMPLIANCE

Streetscape and street improvements outlined in Section 4.7 and in Chapter 5.0, respectively, will reduce congestion, provide increased on-street parking, improve bicycle facilities, and create a safe pedestrian environment. In addition, specific street standards are proposed for the plan area streets which are more reflective of the neighborhood characteristics.

GOAL 3: The City of Encinitas will promote the use of other modes of transport to reduce the dependence on the personal automobile. (Coastal Act/30252)

POLICY 3.1: The needs of the handicapped will be considered in new development plans including handicapped parking, loading, etc.

POLICY 3.2: Continue to assist in expanding public transportation and emphasize public transportation in future development with preference given to cost-effective alternatives. (Coastal Act/30252)

POLICY 3.3: Create a safe and convenient circulation system for pedestrians. (Coastal Act/30252)

POLICY 3.4: Cooperate with San Diego County, SANDAG, and other jurisdictions to help plan and implement a regional multi-modal transportation system that is accessible to residents in the City. (Coastal Act/30252)

POLICY 3.6: The City should provide and encourage efficient links between possible rail transit service and other transportation modes, including rerouting of bus service to interface with transit stops.

POLICY 3.8: A program shall be developed to install sidewalks or paths, where appropriate, around schools, churches, active parks, commercial and other areas of pedestrian activity where public safety or welfare is at issue (Coastal Act/30252)

POLICY 3.10 Standards shall be established for the location and design of newsracks, signs, walls and other improvements which encroach into the public right-of-way. (Coastal Act/30251)

POLICY 3.11: The City will strive to implement a safe, direct, and convenient circulation system for commuting and recreational bicycle traffic. The City will support the development of additional bicycle facilities in the Coastal Zone, including the following:

- all Circulation Element roads will include provisions for bicycle lanes unless precluded by design and safety considerations in which cases, alternative routes shall be provided to form a continuous network.
- the provision of secure bicycle storage facilities at all beaches designated for high and moderate levels of use; and
- the installation of bicycle and surfboard racks on all buses serving the Coastal Zone. (Coastal Act/30252)

SPECIFIC PLAN COMPLIANCE

Streetscape and street improvements outlined in Section 4.7 and in Chapter 5.0, respectively, will improve bicycle facilities and create a safer pedestrian environment.

GOAL 4: **The City should make every effort to develop a circulation system that highlights the environmental and scenic amenities of the area. (Coastal Act/30251)**

POLICY 4.1: Design roads to enhance scenic areas. (Coastal Act/30251)

POLICY 4.2: Promote and encourage roadside and median landscaping. (Coastal Act/30251)

POLICY 4.3: Separate pedestrian, bicycle, and vehicular traffic by encouraging adequate space for walking and biking by striping roadways, excepting freeways. Coastal Act/30252)

POLICY 4.4: Where possible, establish a separate system of hiking trails, bicycle paths and equestrian trails from which motorized vehicles shall be banned.

POLICY 4.9: Prohibit whenever legally possible, or strictly regulate billboards on city streets, roads, freeways, railroad rights-of-way, and lagoons. (Coastal Act/30251/30253)

POLICY 4.10: Develop street lighting standards, where appropriate, consistent with neighborhood/community character and night sky viewing.

POLICY 4.11: Keep street lighting, curbs, and gutter requirements consistent with individual neighborhood character.

POLICY 4.12: Encourage undergrounding of utilities within street rights-of-way and transportation corridors. (Coastal Act/30251)

POLICY 4.14: Where feasible, minimize the dependence on private motor vehicles. (Coastal Act/30252)

SPECIFIC PLAN COMPLIANCE

As mentioned above, a linear park with a multi-modal recreational path extending from La Costa Avenue to Encinitas Boulevard is proposed within the railroad right-of-way.

Streetscape and street improvements outlined in Section 4.7 and in Chapter 5.0, respectively, will reduce congestion, provide increased on-street parking, improve bicycle facilities, and create a safer pedestrian environment. In addition, specific street standards are proposed for plan area streets, which are more reflective of the neighborhood characteristics.

Vulcan Avenue and North Highway 101 have been identified as streets within the specific plan area that require the undergrounding of utilities (see Chapter 10.0).

The proposed mixed use development in the specific plan area will provide more opportunities for live/work situations to occur, as well as residential uses which are closer to the commercial and office professional uses. This will help reduce dependence on the private motor vehicle.

9.4 PUBLIC SAFETY

GOAL 1: Public health and safety will be considered in future land use planning. (Coastal Act/30253)

POLICY 1.4: Develop a master plan for drainage and flood control. (Coastal Act/30236)

POLICY 1.6: The City shall provide for the reduction of unnatural causes of bluff erosion, as detailed in the Zoning Code, by:

- a. Only permitting public access stairways and no private stairways, and otherwise discouraging climbing upon and defacement of the bluff face;
- b. Improving local drainage systems to divert surface water away from the bluff;
- c. Studying the underground water system and looking for potential solution to bluff instability/erosion caused by such water;
- d. Reducing the infusion of ground water from domestic sources through, among other actions, requiring the removal of existing irrigation systems within forty feet of the bluff edge and prohibiting the installation of such systems in new development;
- e. Permitting pursuant to the Coastal Bluff Overlay Zone, bluff repair and erosion control measures on the face and at the top of the bluff that are necessary to repair human-caused damage to the bluff, and to retard erosion which may be caused or accelerated by land-based forces such as surface drainage or ground water seepage, providing that no alteration of the natural character of the bluff shall result from such measures, where such measures are designed to minimize encroachment onto beach areas through an alignment at and parallel to the toe of the coastal bluff, where such measures receive coloring and other exterior treatments and provided that such measures shall be permitted only when required to serve coastal-dependent uses or to protect existing principal structures or public beaches in danger from erosion, and when designed to eliminate or mitigate adverse impacts on local shoreline sand supply; and
- f. Requiring new structures and improvements to existing structures to be set back 25 feet from the inland blufftop edge , and 40 feet from coastal bluff-top edge with exceptions to allow a minimum coastal bluff-top setback of no less than 25 feet. For all development proposed on coastal bluff-tops, a site-specific geotechnical report shall be required. The report shall indicate that such a reduced setback will not result in risk of foundation damage resulting from bluff erosion or retreat to the structure within its economic life and with other engineering evidence to justify the coastal bluff-top setback.

In all cases, all new construction shall be specifically designed and constructed such that it could be removed in the event of endangerment and the applicant shall agree to participate in any comprehensive plan adopted by the City to address coastal bluff recession and shoreline erosion problems in the City.

This does not apply to minor structures that do not require a building permit, except that no structures, including walkways, patios, patio covers, cabanas, windscreens, sundecks, lighting standards, walls, temporary accessory buildings not exceeding 200 square feet in area, and similar structures shall be allowed within five feet from the bluff top edge; and

- g. Permanently conserving the bluff face within an open space easement or other suitable instrument. (Coastal Act/30210/30235/30240/30251/30253)

Standards for the justification of preemptive erosion control devices and limits on location of shoreline devices shall be as detailed in the Zoning Code.

- POLICY 1.15: The City shall establish and implement standards, based on the 50- or 100-year storm, for flood control and drainage improvements, and the maintenance of such improvements, designed to assure adequate public safety. Such standards and improvements shall be consistent with the policies of this Plan to respect community character and maintain natural or natural-appearing drainage courses whenever feasible.

SPECIFIC PLAN COMPLIANCE

Development within the Specific Plan area shall comply with Sections 30.34.020 and 30.34.030 of Title 30 of the Encinitas Municipal Code.

9.5 RESOURCE MANAGEMENT

- GOAL 1:** The City will conserve, protect, and enhance the water resources in the planning area. (Coastal Act/30231)

- POLICY 1.1: Require new development to utilize measures designed to conserve water in their construction.

- POLICY 1.10: Promote the use of water efficient sprinkling and gardening systems to include ordinances and technology to encourage drought tolerant plants.

SPECIFIC PLAN COMPLIANCE

General landscape recommendations have been established which address water conservation (see Section 4.8).

- GOAL 3:** The City will make every effort possible to preserve significant mature trees, vegetation and wildlife habitat within the planning area. (Coastal Act 30240)

- POLICY 3.1: Mature trees of community significance cannot be removed without City authorization.

- POLICY 3.2: Mature trees shall not be removed or disturbed to provide public right-of-way improvements if such improvements can be deferred, redesigned, or eliminated. This policy is not meant to conflict with the establishment of riding/hiking trails and other natural resource paths for the public good, or with the preservation of views.

- POLICY 3.3: The City will examine ways to aesthetically trim street trees and vegetation within the public right-of-way including the possibility of using contract services or City personnel. (Coastal Act/30240/30251)

- POLICY 3.4: A program shall be developed to trim roots and replace sidewalks and other public facilities which may be damaged by roots. (Coastal Act/30251/30254)
- POLICY 3.5: A street tree planting program shall be developed and implemented.
- POLICY 3.6: Future development shall maintain significant mature trees to the extent possible and incorporate them into the design of development projects.

SPECIFIC PLAN COMPLIANCE

An objective of the specific plan is to establish a street tree master plan which includes preservation of mature trees, planting additional street trees, and a regular maintenance program. Landscape recommendations (see Section 4.8) have been established for the specific plan area which encompass street tree design/planting and management/maintenance recommendations.

GOAL 4: The City, with the assistance of the state, federal and regional agencies, shall provide the maximum visual access to coastal and inland views through the acquisition and development of a system of coastal and inland vista points. (Coastal Act/30251)

POLICY 4.5: The City will designate "Scenic/Visual Corridor Overlay" areas within which the character of development would be regulated to protect the integrity of the Vista Points according to the following criteria:

- Critical viewshed areas should meet the following requirements:
 - extend radially for 2,000 feet (610M) from the Vista Point; and
 - cover areas upon which development could potentially obstruct, limit, or degrade the view.
- Development within the critical viewshed area should be subject to design review based on the following:
 - building height, bulk, roof line and color and scale should not obstruct, limit or degrade the existing views;
 - landscaping should be located to screen adjacent undesirable views (parking lot areas, mechanical equipment, etc.) (Coastal Act/30251/30253)

POLICY 4.6: The City will maintain and enhance the scenic highway/visual corridor viewsheds. (Coastal Act/30251)

POLICY 4.7: The City will designate the following view corridors as scenic highway/visual corridor viewsheds:

- Highway 101, from Encinitas Blvd. to La Costa Ave.
- Leucadia Blvd. between Highway 101 and El Camino Real

POLICY 4.9: It is intended that development would be subject to the design review provisions of the Scenic/ Visual Corridor Overlay Zone for those locations within Scenic View Corridors, along scenic highways and adjacent to significant viewsheds and vista points with the addition of the following design criteria:

- Road Design
 - Type and physical characteristics of roadway should be compatible with natural character of corridor, and with the scenic highway function.
- Development Design
 - Building and vegetation setbacks, scenic easements, and height and bulk restrictions should be used to maintain existing views and vistas from the roadway.
 - Off-site signage should be prohibited and existing billboards removed.
 - Development should be minimized and regulated along any bluff silhouette line or on adjacent slopes within view of the lagoon areas and Escondido Creek.
 - Where possible, development should be placed and set back from the bases of bluffs, and similarly, set back from bluff or ridge top silhouette lines; shall leave lagoon areas and floodplains open, and shall be sited to provide unobstructed view corridors from the nearest scenic highway.
 - Development that is allowed within a viewshed area must respond in scale, roof line, materials, color, massing, and location on site to the topography, existing vegetation, and colors of the native environment. (Coastal Act/30251/30253)

SPECIFIC PLAN COMPLIANCE

A recreation/open space objective of the specific plan includes the development of a linear park within the railroad right-of-way which fronts along North Highway 101 (see Section 4.7). Designated existing vista points and view corridors, specifically North Highway 101 through Leucadia, shall comply with Chapter 30.32 and Section 30.34.080 of Title 30 of the Encinitas Municipal Code respectively.

GOAL 5: The City will make every effort to participate in programs to improve air and water quality in the San Diego region. (Coastal Act/30231)

SPECIFIC PLAN COMPLIANCE

The introduction of mixed use development in the specific plan area will provide more opportunities for live/work situations to occur, as well as residential uses which are closer to the commercial uses. This should reduce automobile trips, therefore, improving future air quality.

GOAL 7: The City will make every effort to ensure significant scientific and cultural resources in the planning area are preserved for future generations. (Coastal Act/30250)

POLICY 7.1: Require that paleontological, historical and archaeological resources in the planning area are documented, preserved or salvaged if threatened by new development. (Coastal Act/30250)

POLICY 7.2: Conduct a survey to identify historic structures and archaeological/cultural sites throughout the community and ensure that every action is taken to ensure their preservation. (Coastal Act/30250/30253(5))

POLICY 7.3: The City will pursue the development of a historic resources program to assist in the identification, preservation, and restoration of those buildings, structures, and places within the City that have historic significance.

SPECIFIC PLAN COMPLIANCE

Paleontological, historical, and archaeological resources shall comply with development standards pursuant to Section 30.34.050 of Title 30 of the Encinitas Municipal Code.

An architectural and historic resources inventory was conducted as part of the specific plan process for the planning area. The historic preservation program (see Chapter 7.0) offers incentives for preservation of historic and architectural resources, rather than establishing new regulatory limitations.

GOAL 9: The City will encourage the abundant use of natural and drought tolerant landscaping in new development and preserve natural vegetation, as much as possible, in undeveloped areas. (Coastal Act/30240/30251)

POLICY 9.1: The City will initiate and pursue the landscaping of appropriate median and parking areas with trees on all new and existing arterial streets. (Coastal Act/30251)

POLICY 9.4: Encourage and adopt standards for the use of drought tolerant and/or natural landscaping and efficient irrigation systems throughout the City. (Coastal Act/30231/30240)

POLICY 9.5: Encourage and adopt standards for clean-up, landscaping, beautification, utility undergrounding, and additional landscaping in parking facilities where needed in existing commercial areas. (Coastal Act/30251/30252)

POLICY 9.6: Require landscaping in the design of new residential, commercial, and industrial areas and buildings as detailed in the City Zoning Code regulations. (Coastal Act/30251/30253)

POLICY 9.7: Encourage and adopt standards for landscaping of existing commercial and industrial uses to screen and buffer unsightly and congested commercial/industrial areas from surrounding land uses. (Coastal Act/30251)

SPECIFIC PLAN COMPLIANCE

General landscape recommendations and design recommendations have been established which address landscape design and use of drought tolerant plants (see Chapter 4.0).

GOAL 13: Create a desirable, healthful, and comfortable environment for living while preserving Encinitas' unique natural resources by encouraging land use policies that will preserve the environment. (Coastal Act/30250/30251)

POLICY 13.3: Encourage the use of buffer zones to separate major thoroughfares from adjacent areas and protect them from pollutants of noise, exhaust, and light. (Coastal Act/30250/30251)

POLICY 13.5: The City shall promote and require the conservation and preservation of natural resources and features of the area in their natural state and avoid the creation of a totally urbanized landscape. Encourage the planting of trees and other vegetation, especially native species, to enhance the environment. (Coastal Act/30240/30251)

SPECIFIC PLAN COMPLIANCE

The preservation of and additional planting of trees along scenic North Highway 101 is a major objective established for the specific plan area. A street tree master plan for North Highway 101 and landscape recommendations have been established for the specific plan area (see Section 4.8.)

9.6 RECREATION

GOAL 1: The maintenance of the open space resources in the planning area will continue to be emphasized. (Coastal Act/30240)

POLICY 1.6: Establish mini-parks and playlots in high density areas where larger parks are inaccessible or impractical to provide, and only when the provision of neighborhood parks to serve local neighborhood park needs is not possible.

POLICY 1.7: Provide a neighborhood park within convenient, and where possible, walking distance for all urban area residents.

POLICY 1.11: Develop an open space program that will link the various communities together with parks, recreation/pedestrian access and natural visual corridors.

POLICY 1.12: Active parks, passive parks, and natural open space shall be provided in each of the communities to the extent possible.

POLICY 1.14: The City shall attempt to acquire the Santa Fe railroad right-of-way, should it become available, for the development of recreational facilities and landscaping as well as transportation and other public needs.

POLICY 1.16: Future trails in addition to those planned for in this element may be added to the existing systems to enhance the recreational opportunities of the City.

SPECIFIC PLAN COMPLIANCE

The nearest developed park sites to the residential areas within the specific plan area are Orpheus Park located at Orpheus Avenue and Union Street and Leucadia Roadside Park located at the west end of Leucadia Boulevard on North Highway 101. However, the Roadside Park is a very small passive park and Orpheus Park is not located within walking distance of most of the residential areas within the planning area.

The proposed linear park within the railroad right-of-way will provide an opportunity to provide needed open space and some active and passive recreational activity such as jogging, bicycling, and walking. The large agricultural properties along Vulcan Avenue also provide an opportunity to develop parks which would be consistent with Policy 1.6 above.

The existing Leucadia Roadside Park has been identified as a significant historic resource by the historic resources inventory and is proposed to be rezoned to a Historic Park Zone in order to protect this resource.

GOAL 2: The City will make every effort to preserve open space areas that represent a significant environmental resource in the community. (Coastal Act/30240)

POLICY 2.2: Provide and maintain an inter-linking network of trails for horseback riding, hiking, and bicycling; and minimize the cost of the trail system by encouraging the use of drainage

channels, flood plains, existing trails, public lands, excess street rights-of-way, and major utility rights-of-way. (Coastal Act/30212.5/30252)

POLICY 2.3: Encourage the preservation and protection of areas for the recreational activities characteristic of Encinitas such as horseback riding, surfing, skin-diving, bicycling, walking, and jogging. (Coastal Act/30212.5/30252)

SPECIFIC PLAN COMPLIANCE

The existing Leucadia Roadside Park has been identified as a significant historic resource by the historic resources inventory and is proposed to be rezoned to a Historic Park Zone in order to protect this resource. Refer to Chapter 3.0 for use and development regulations.

The proposed linear park within the railroad right-of-way will provide an opportunity to provide needed open space and some active and passive recreational activity such as jogging, bicycling, and walking. The multi-modal recreational path within the linear park will provide linkage to a regional trail system both to the north and to the south of Encinitas as well as provide linkage to other recreational destinations within the surrounding area such as beach access points and Moonlight State Beach and Park.

9.7 NOISE

GOAL 1: Provide an acceptable noise environment for existing and future residents of the City of Encinitas.

POLICY 1.1: Review actions or projects that may have noise generation potential to determine what impact they may have on existing land uses. If a project would cause an increase in traffic noise levels; the policy of the City of Encinitas is to accept an increase up to an Ldn of 55 dB in outdoor residential use areas without mitigation. If a project would increase the traffic noise level by more than 5 dB and the resulting Ldn would be over 55 dB, then mitigation measures must be evaluated. If the project, or action, would increase traffic noise levels by 3 dB or more and the resulting Ldn would exceed 60 dB in outdoor use areas in residential development, noise mitigation must be similarly evaluated. The impact of non-transportation projects must generally be evaluated on a case-by-case basis. The following recommendations will aid in evaluating the impacts of commercial and industrial projects.

- a) Performance Standards Adjacent to Residential Areas. New commercial construction adjacent to residential areas should not increase noise levels in a residential area by more than 3 dB (Ldn) or create noise impacts which would increase noise levels to more than an Ldn of 60 dB at the boundary of the nearest residential area, whichever is more restrictive.
- b) Performance Standards Adjacent to Commercial and Industrial Areas. New commercial projects should not increase noise levels in a commercial area by more than 5 dB (Ldn) or increase noise levels to an Ldn in excess of 70 dB (office buildings, business and professional) or an Ldn of 75 dB (industrial) at the property line of an adjacent commercial/industrial use, whichever is more restrictive.

These criteria may be waived if, as determined by a noise analysis, there are mitigating circumstances (such as higher existing noise levels) and/or no uses would be adversely affected. Where conditions are unusual or where backgrounds are unusually low and the characteristics of a new noise source are not adequately described by using the Ldn noise descriptor, additional acoustical analysis is encouraged and the conclusions of such analysis will be considered by the City.

- POLICY 1.2:** An Ldn of 60 dB is the maximum acceptable outdoor noise level in residential outdoor use areas. The City recognizes that there are residential areas in which existing noise levels exceed an acceptable level. The City will adopt a Noise Wall/Barrier Installation Policy for determining which areas should receive soundwalls along the major street system and to evaluate possible cost participation programs for constructing these soundwalls.
- POLICY 1.4:** The City will limit truck traffic in residential and commercial areas to designated truck routes. Limit construction, delivery, and through truck traffic to designated routes. Distribute maps of approved truck routes to City traffic officers.
- POLICY 1.5:** The City will establish and maintain coordination among City, County, and State agencies involved in noise abatement and other agencies to reduce noise generated from sources outside the City's jurisdiction.
- POLICY 1.7:** Apply Title 24 of the California Administrative Code, associated with noise insulation standards, to single-family dwellings.

SPECIFIC PLAN COMPLIANCE

Future development within the specific plan area shall be required to comply with Title 24 of the California Administrative Code and other citywide regulations.

- GOAL 2:** Require that new development be designed to provide acceptable indoor and outdoor noise environments.

SPECIFIC PLAN COMPLIANCE

Future development within the specific plan area shall be required to comply with Title 24 of the California Administrative Code and other citywide regulations.

- GOAL 3:** Ensure that residents are protected from harmful and irritating noise sources to the greatest extent possible.

SPECIFIC PLAN COMPLIANCE

Future development within the specific plan area shall be required to comply with Title 24 of the California Administrative Code and other citywide regulations.

- GOAL 4:** Provide for measures to reduce noise impacts from stationary noise sources.

- POLICY 4.1:** Ensure inclusion of noise mitigation measures in the design and operation of new and existing development.

SPECIFIC PLAN COMPLIANCE

Future development within the specific plan area shall be required to comply with Title 24 of the California Administrative Code and other citywide regulations.

Blank Page

10.0 IMPLEMENTATION

10.1 SPECIFIC PLAN ADMINISTRATION

10.1.1 PURPOSE

The North 101 Corridor Specific Plan provides implementation guidelines for land use and development quality. The purpose of this section is to describe the procedures required for the timely implementation of development within the specific plan area.

A few documents have been prepared and processed concurrently with the adoption of the North 101 Corridor Specific Plan which include a general plan amendment, local coastal program amendment, zoning map and ordinance amendments, and certification of an environmental impact report (EIR) and addendum. These documents will form the basic framework to guide future development within the specific plan area.

10.1.2 PROCESSING AND REVIEW

Future development within the specific plan area will involve obtaining the necessary development permits for the division of a parcel of land into two or more parcels; the construction, reconstruction, conversion, structural alteration, relocation, or enlargement of any structure; any mining, excavation, landfill, or land disturbance, and any use or extension of the use of land. City review of these permit applications will ensure consistency of the proposed improvements with the design recommendations and development regulations outlined in the specific plan.

The specific plan area shall be developed in accordance with the criteria outlined in this specific plan and accompanying documents and in accordance with other land use and zoning regulations of the City of Encinitas. In cases where discrepancies occur between the specific plan and citywide development standards, the development regulations contained in the North 101 Corridor Specific Plan shall prevail. All development within the specific plan boundary shall be consistent with the North 101 Corridor Specific Plan. The development procedures are as follows:

A. Development Applications (except Design Review)

Development applications shall be processed according to the procedures indicated in Title 30 of the Encinitas Municipal Code.

B. Design Review

Design Review applications shall be processed according to the procedures indicated in Chapter 23.08 of the Encinitas Municipal Code. Also reference Chapter 4 of the specific plan for additional information on applications subject to or exempt from Design Review.

C. Specific Plan Amendments

Amendments to the specific plan shall require a modification to the specific plan and shall be subject to the procedures as indicated in Chapter 30.72 of the Encinitas Municipal Code.

D. Nonconformities

With the adoption of the specific plan, any nonconforming use shall be subject to the provisions as indicated in Chapter 30.76 of the Encinitas Municipal Code. A "nonconforming use" is a use that:

1. Is not within the scope, either expressly or implicitly, of the zoning restrictions set forth in this specific plan that announce the purpose, intent, permissible uses, accessory use and prohibited uses for the zone in which the particular use is located;
2. Did comply with the zoning restrictions contained in the zoning ordinance in effect at the time the use was created as was lawfully created; and
3. Has not been terminated in accordance with the provisions of the specific plan.

10.2 IMPROVEMENTS PROGRAM

Certain basic financing needs have been identified which include improvements to traffic/circulation, drainage, and utility infrastructure. Other specific capital improvements include streetscape enhancement, community facilities, and community development programs.

The following table summarizes an initial estimate of those capital needs and costs that apply within the North 101 Corridor Specific Plan area. These are preliminary cost estimates based on '96 dollars. The cost estimates for the undergrounding of utilities, drainage, and street improvements can be reduced if these projects are coordinated and implemented at the same time.

Improvement Projects

Undergrounding Utilities

North Highway 101 (between La Costa Ave. and Encinitas Blvd.)	\$4,400,000
Vulcan Avenue (between La Costa Ave. and Encinitas Blvd.)	2,970,000

Infrastructure

North Highway 101 Drainage Improvement Project	4,650,000
Street improvements - curb/gutter, sidewalk, paving, striping, median, landscaping, street furniture, lighting, etc. (North Highway 101, North Vulcan Avenue and side streets)	5,220,000
Alley improvements	555,000

Community Facilities

Linear Park/Coastal Rail Trail project	2,000,000
Leucadia Roadside Park streetscape improvement	60,000

Community Development

Facade Grant Program (N. Hwy. 101)

200,000

Total Preliminary Cost Estimate:**20,055,000****10.3 FINANCING STRATEGIES**

A detailed financing plan should be prepared in order to successfully implement the improvements and programs proposed by the North 101 Corridor Specific Plan. Along with establishing specific goals and policies, the financing plan should analyze a series of methods to finance infrastructure and other improvements, recommend preferred alternatives, and develop a process for enacting financing methods.

The following is a summary of possible methods for financing the specific plan improvements as identified above. Some of these financing methods may be impacted by the passage of Proposition 218 in November of 1996. However, the extent of impact will not be known until Proposition 218 is completely implemented.

10.3.1 SPECIAL ASSESSMENT DISTRICTS (1911, 1913, 1915 ACT)

California law provides procedures to levy assessments against benefiting properties and issue tax exempt bonds to finance public facilities and infrastructure improvements. Assessment districts, also known as improvement districts, are initiated by the legislative body (e.g. city), subject to majority protest of property owners or registered voters. Assessments are distributed in proportion to the benefits received by each property, and represent a lien against property. The assessments are fixed dollar amounts, and may be prepaid. Only improvements with property-specific benefits (e.g. roads, and sewer and water improvements) may be financed with assessments.

10.3.2 AREA OF BENEFIT FEES

Area of benefit fees may be enacted by the legislative body (i.e. city) through adoption of an ordinance, without voter approval. The fee must be directly related to the benefit received. It does not create a lien against property, but must be paid in full as a condition of approval. Its principle use is for encumbering properties that do not voluntarily enter into an assessment of a Community Facilities District (CFD), so that they pay their fair share at the time they are ready to be developed. Proceeds may be used to reimburse property owners who pay up-front cost for facilities benefiting other properties. Benefiting properties may be given the option to finance the fees by entering into an assessment district (1913/1911 Act or Mello-Roos CFD).

10.3.3 MELLO-ROOS COMMUNITY FACILITIES DISTRICTS

The Mello-Roos Community Facilities Act of 1982 allows for the creation of special districts authorized to levy a special tax and issue tax exempt bonds to finance public facilities and services. A CFD may be initiated by the legislative body or by property owner petition, and must be approved by a 2/3 majority of either property owners or registered voters (if there are more than 12 registered voters living in the area).

Taxes are collected annually with property taxes, and may be prepaid if prepayment provisions are specified in the tax formula. The levy creates a tax lien against the property. There is no requirement that the tax be apportioned on the basis of benefit. Because there is no requirement to show special benefit, Mello-Roos levies may be used to fund improvements of general benefit, such as fire and police facilities, libraries and parks, as well as improvements that benefit specific properties. The provision also allows for the reallocation of cost burdens to alleviate untenable burdens on specific properties.

10.3.4 LANDSCAPING AND LIGHTING DISTRICTS

Landscaping and Lighting Districts (LLD) may be used for installation, maintenance and servicing of landscaping and lighting, through annual assessments on benefiting properties. LLD's also may provide for construction and maintenance of appurtenant features, including curbs, gutters, walls, sidewalks or paving, and irrigation or drainage facilities. They also may be used to fund and maintain parks above normal park standards maintained from general fund revenues.

10.3.5 STATE COMMUNITY REDEVELOPMENT LAW

This Act allows communities to utilize tax increment financing to carry out redevelopment activities, by applying tax increments obtained in the project area to finance planning, administrative, acquisition, and improvement activities. The Act permits a redevelopment agency to finance land acquisition for public purposes, construction of public facilities, such as roads, parks, and sewers, and administrative, legal, planning, and engineering costs related to the project.

The agency could issue bonds to finance project area improvements and administrative costs, and could apply the tax increments derived in the project area to pay the debt service on the bonds. Tax increments are those tax revenues produced in an area in excess of the revenues produced at the time the Redevelopment Agency is formed. The excess revenues thus produced can be used to pay off bonds used to finance the expenses of the redevelopment process such as administration, planning, acquisition, and construction of public facilities. Current and projected development could provide a substantial revenue base from which to finance major improvements.

10.3.6 SDG&E UNDERGROUNDING FUNDS

Utility companies are required to budget funds each year for undergrounding. These budgets are approved by the Public Utilities Commission and assigned to specific projects in each area based on priorities developed by local government.

10.3.7 SURFACE TRANSPORTATION PROGRAM (STP) FUNDS

The passage of the Intermodal Surface Transportation Efficiency Act of 1991 provides funding to strengthen the national transportation system through "enhancement" projects. Transportation enhancement activities include: pedestrian and bicycle facilities, acquisition of scenic and historic sites, scenic and historic highway programs, landscaping, rehabilitation of historic transportation facilities, preservation of abandoned transportation corridors, archeological planning and research, control and removal of outdoor advertising, and mitigation of water quality impacts from roadway runoff. Funding can be obtained through San Diego Association of Governments (SANDAG) on a regional basis and also directly through the State.

10.3.8 COMMUNITY DEVELOPMENT BLOCK GRANTS (CDBG)

These grants issued from the Federal Housing and Urban Development Department (HUD) are available to areas in which at least 50 percent of the households have a low-moderate income.

10.3.9 OTHER FUNDING SOURCES

There may be other sources available to finance improvement projects such as special assessment districts, government grants, or various types of bonds not listed above, that may be used to fund improvements.

Blank Page

U.C. BERKELEY LIBRARIES



C101692627